



Interoffice Memo
Office of Design Policy & Support

DATE: 1/8/2021

FILE: P.I.# 0015049
Cobb County / GDOT District 7 - Metro Atlanta
Rottenwood Creek Trail - Phase II - Scoping Only

FROM: *Dane Peters*
for R. Christopher Rudd, PE, State Design Policy Engineer

TO: SEE DISTRIBUTION

SUBJECT: APPROVED CONCEPT REPORT

Attached is the approved Concept Report for the above subject project.

Attachment

Distribution:

Hiral Patel, Director of Engineering
Joe Carpenter, Director of P3
Albert Shelby, Director of Program Delivery
Carol Comer, Director, Division of Intermodal
Darryl VanMeter, Assistant Director of P3/State Innovative Delivery Administrator
Matthew Markham, Deputy Director of Planning
Kim Nesbitt, Program Delivery Administrator
Bobby Hilliard, Program Control Administrator
Eric Duff, State Environmental Administrator
Donn Digamon, State Bridge Engineer
Andrew Heath, State Traffic Engineer
Angela Robinson, Financial Management Administrator
Erik Rohde, State Project Review Engineer
Monica Fournoy, State Materials Engineer
Patrick Allen, State Utilities Engineer
Eric Conklin, State Transportation Data Administrator
Attn: Systems & Classification Branch
Benny Walden, Statewide Location Bureau Chief
Kathy Zahul, District Engineer
Paul DeNard, District Preconstruction Engineer
Shun Pringle, District Utilities Manager
Olusola Adekonojo, Project Manager
BOARD MEMBER - 11th Congressional District



Project Concept Report

Project Type: Enhancement P.I. Number: 0015049
 GDOT District: 7 County: Cobb
 Federal Route Number: N/A State Route Number: 280
 Project Number: N/A

The proposed project is to create a shared use path along Franklin Gateway to SR 280 (Delk Road (SR 280)) and along Delk Road (SR 280) crossing over I-75 via ped bridge. The shared use path continues to along Rottenwood Creek to Delk Road and terminates at Bentley Road. ADA accessible ramps, crosswalks at intersections, landscaping, lighting, seating areas and pedestrian enhancements will be included in this project.

Submitted for approval: *Stephen Hopper* * **Concept Report updated 12/15/2020**

Stantec-Stephen Hopper 7/13/2020
Date

Marc Simmons 7/13/20
Date

City of Marietta-Marc Simmons *Kimberly W. Nesbitt* 7/27/2020
Date

State Program Delivery Administrator *Olusola Adekunjo* *KEED* 07 - 14 - 2020
Date

GDOT Project Manager * **Recommendations are on file ~ OB**

Recommendation for approval * **Eric Duff** 7/29/2020
Date

State Environmental Administrator 8/11/2020
Date

* **Chris Raymond** 8/11/2020
Date

for State Traffic Engineer 8/03/2020
Date

* **Erik Rohde** 8/10/2020
Date

Project Review Engineer 8/10/2020
Date

for State Utilities Engineer 8/12/2020
Date

* **Paul DeNard** 8/12/2020
Date

for District Engineer 8/27/2020
Date

* **Bill DuVall - no comments** 8/27/2020
Date

State Bridge Engineer 8/27/2020
Date

- MPO Area: This project is consistent with the MPO adopted Regional Transportation Plan (RTP)/Long Range Transportation Plan (LRTP).
- Rural Area: This project is consistent with the goals outlined in the Statewide Transportation Plan (SWTP) and/or is included in the State Transportation Improvement Program (STIP).

Charles A. Robinson Digitally signed by Charles A. Robinson
DN: cn=US, E=chrobinson@dot.ga.gov, o=Georgia
Department of Transportation, OU=Office of Planning,
CN=Charles A. Robinson
Date: 2020.08.06 09:01:46-0400 8/6/2020
for State Transportation Planning Administrator Date

* **Recommendations were also received from the following: ~ OB**
Office of Intermodal: Alan C. Hood (10/16/20)

State Transportation Planning Administrator

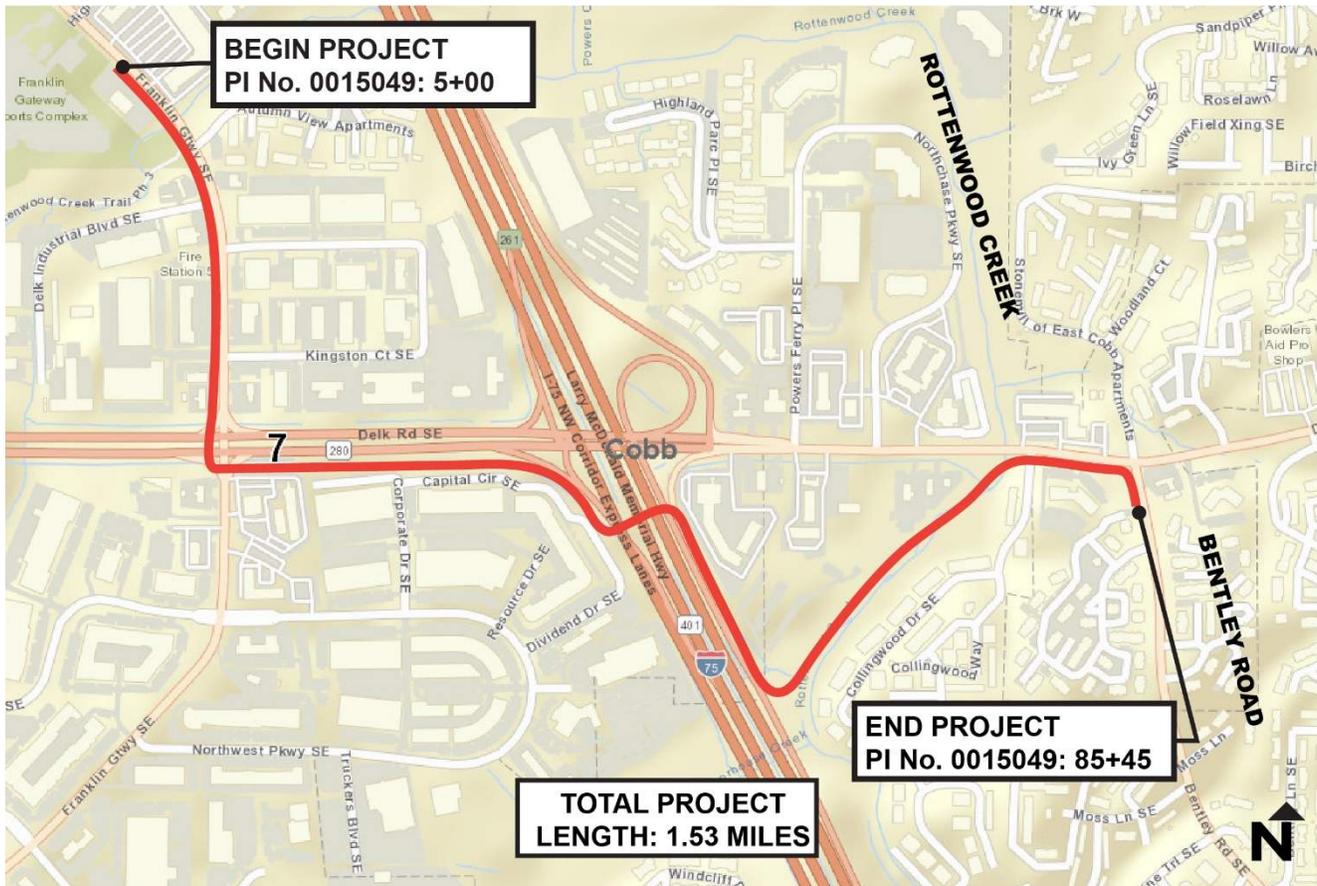
Date

PROJECT LOCATION MAP

(Not to Scale)

Location Map for PI0015049, Cobb County, SR 280

Rottenwood Creek Trail - Phase II – Scoping Only



PLANNING AND BACKGROUND

Prepared By: *Stantec*

Date: *4/2/2020*

Office: *GDOT Traffic Operations*

Date Approved: *8/3/2020*

The project will address elements of pedestrian mobility and connectivity along the corridor, streetscape enhancements, public realm gathering spaces, social nodes, mobility improvements, and bike/ped accessibility. Pedestrian crosswalks are proposed at several locations, including Delk Industrial Boulevard, Delk Road (SR 280), Franklin Gateway and Collingwood Drive. A reconfiguration of the Delk Road (SR 280) crosswalk is also considered to provide more areas of refuge as well as shorter crossing distances and greater awareness to crossing pedestrians.

The City of Marietta has a trails Master plan and inventory that includes a system of trails. There is an existing north/south trail corridor that connects Kennesaw Mountain down to the Chattahoochee River. This project will serve as the second phase to the east/west trail corridor that connects the north/south corridor to the farthest areas of East Marietta. Marietta has future plans to connect these trails to connecting trails to Kennesaw and Woodstock.

The path will move away from the roadway and near Rottenwood Creek on the east side of I-75 and south of Delk Road. The path in this area will be along the natural corridor and creek and be placed mainly in the exiting utility easement area. The path will provide direct access to the creek from the pathway. There will be one crossing over Rottenwood Creek on a pedestrian bridge near Delk Road that will then progress on to create the pathway on the Delk Road East of I-75 to Bentley Road. The proposed route of Rottenwood Creek Trail intersects with a dense residential area in need of pedestrian connectivity from the Bentley Road Corridor.

Where the path terminates at Bentley Road, Cobb County has plans to extend this path to the new Windy Hill Connector at Terrell Mill as part of their overall future trails plan. This will complete the connection of Marietta to the Chattahoochee River via Bob Callen Trail.

The project limits are along Franklin Gateway at the driveway entrance to Mismoj Properties, LLC south to Delk Road (SR 280), along the south side of Delk Road (SR 280) and across I-75, through the Rottenwood Creek riparian corridor and along the south side of Delk Road to Bentley Road. Impacts to the side streets that intersect the main corridor will be limited to the spatial requirements necessary to connect pedestrian pathways and create accessible landings and crossings. Access management of driveways along the trail corridor will be analyzed and considered for potential closures to provide a corridor that has less vehicular and pedestrian conflict points. Pedestrian lighting is proposed along the path and pedestrian bridge.

The path will target opportunities for interaction with nearby residents and visitors to the area by evaluating bus stop locations and trail heads for people to access the trail. The extension of Phase II connected to Phase I will give people direct walkable and bikeable access to the Cobb County Transit Station on South Marietta Parkway and other trails networks.

The pedestrian bridge over I-75 will be a placemaking gateway that identifies both the trail and the City of Marietta. The pedestrian bridge design will be coordinated with GDOT, FHWA, and SRTA. The pedestrian bridge will go under the Northwest Corridor and then cross over I-75 with all minimum allowable clearances. There will be another pedestrian bridge that crosses over Rottenwood Creek where the Delk Road bridge goes over Rottenwood Creek.

For the trail crossing at Delk Road at Franklin Gateway, the design considers alternatives to provide a pedestrian refuge at the existing median to reduce the 100-foot crossing of the intersection. Timings of the pedestrian signals,

median size, and crosswalk orientation will all be considered to bring the greatest visibility and accessibility to the pedestrian in the crosswalk.

The shared-use path will adhere to all GDOT guidelines when the path is along both state and local roads. When the path is along the natural alignment in the woods and adjacent to Rottenwood Creek, the path will abide by PROWAG standards.

The measurable goals of the project will be to increase and extend mobility opportunities along the corridor and throughout the City of Marietta including biking and walking along the corridor, reducing vehicular/pedestrian conflicts, and providing high visibility, wider, and accessible connections from the neighborhoods to the project corridor.

Existing conditions:

1. Franklin Gateway-The corridor is a four-lane road that includes four turn lanes exiting on to Delk Road (SR 280) and is classified as a Local Road and Street. The posted speed is 35 miles-per-hour. There is a major intersection with Delk Road (SR 280). The travel lanes are 11 feet and turn lanes are 10 feet wide each. There is little to no buffer between sidewalk and travel lane. Sidewalks are in generally good condition but vary in width and conflict with several curb cut driveways. Crossing over the heavily travelled Delk Road creates significant pedestrian and vehicular conflict by crossing over four turn lanes and six travel lanes from one side of Delk Road to the other.
2. Delk Road (west of I-75) – The portion of Delk Road west of I-75 is a six-lane state route that includes four turn lanes and is classified as a Minor Arterial. The posted speed limit is 55 miles-per-hour. There is a major intersection with Franklin Gateway. The travel lanes and turn lanes are 12 feet wide. There is an 11-foot shoulder on either side and 17-feet wide raised concrete median. There are existing sidewalks on each corner of the Delk Road / Franklin Gateway intersections, but those sidewalks do not extend beyond the intersection. There are no sidewalks along the length of Delk Road prior to I-75. Delk Road passes under the Northwest Corridor before crossing over I-75.
3. Interstate 75 – The trail will pass over the main I-75 corridor, on/off ramps, and the eastern sound wall. The trail will pass under the Northwest Corridor and over I-75 via pedestrian bridge. The south bound on/ramp is metered and design of the pedestrian bridge will take metered location and stop bar into consideration. There are two south bound on-ramps, 6 south bound lanes, 6 north bound lanes, and 2 off ramps that the pedestrian bridge will cross over. The two north bound off ramps are elevated above the I-75 north bound lanes.
4. Natural corridor along Rottenwood Creek- The natural corridor surrounding Rottenwood Creek varies in width from 250 to 300 feet wide. It is a highly vegetated corridor with the path going from Southwest to Northeast from I-75 to Delk Road. Rottenwood Creek is a wide, flowing stream with wide sandy beaches and steep embankments. The natural corridor provides a buffer zone between the residential area to the Southeast and the commercial zone along Delk Road to the Northwest. While somewhat ignored, the green corridor that encompasses Rottenwood Creek is a beautiful green corridor.
5. Delk Road (east of I-75) – The corridor at the project area is made of four 11-foot travel lanes and a 14-foot center turn lane and is classified as a Minor Arterial. This section of Delk Road is not part of the state route system. posted speed limit on this section of Delk Road is 45 miles-per-hour. There is a major intersection with Bentley Road. There are existing sidewalks along both sides of this section of Delk Road that are 5-feet wide with a 2-foot grass buffer. There are numerous pedestrian and vehicular conflicts along the corridor where sidewalks cross signalized intersections and driveway curb cuts. The sidewalk gets pushed against the curb where Delk Road crosses over Rottenwood Creek bringing the pedestrian closer to fast-moving vehicular traffic.

Other projects in the area:

1. 0012873-ROTTENWOOD CREEK TRAIL - PHASE I - SCOPING ONLY
 - a. Termination of trail of Phase 1 to be coordinated with Beginning of Phase II including public plaza space and connections.
2. 760147- WINDY HILL ROAD WIDENING @ ROTTENWOOD CK

MPO: Atlanta TMA **TIP #:** N/A
Congressional District(s): 11

Federal Oversight: PoDI Exempt State Funded Other

Projected Traffic: 24 HR T: 3.5 % (Delk Road) Current Year (2019): 34,100 (Delk Road)

This is not a proposed roadway project. Existing truck and traffic data are shown to demonstrate the volumes that correspond to the impacts of the shared-use path. Existing traffic data volumes and turning movements can be found in Attachment 6.

AASHTO Functional Classification (Delk Road): *Minor Arterial*

AASHTO Context Classification (Delk Road): Suburban

AASHTO Project Type (Delk Road): New Construction

Is the project located on a NHS roadway? No Yes

The project does cross over I-75 which is an NHS roadway.

Complete Streets - Bicycle, Pedestrian, and/or Transit Standard Warrants:

Warrants met: None Bicycle Pedestrian Transit

Bicycle Warrants Standards: #1 and #3

Pedestrian Warrants Standards: #1, #2, and #4

Transit Warrants Standards: #1 and #2

See Attachment 11.a for Complete Streets Warrants.

Is this a 3R (Resurfacing, Restoration, & Rehabilitation) Project? No Yes

Pavement Evaluation and Recommendations

Initial Pavement Evaluation Summary Report Required? No Yes

Feasible Pavement Alternatives: HMA PCC HMA & PCC

Is the project located on a Special Roadway or Network? No Yes

Is the project located on or intersect an RTOP corridor? No Yes

Is Federal Aviation Administration coordination anticipated? No Yes

Per the Federal Aviation Criteria, the proposed shared use path requires study and coordination due to location to the Dobbins Air Force Base and a navigation facility..

The following note shall be added to the construction plans:

ANY VERTICAL CONSTRUCTION EQUIPMENT, SUCH AS CRANES, IN EXCESS OF 30 FEET ABOVE THE ROADWAY ELEVATION MUST BE EVALUATED BY THE FAA. EVALUATION BY FILING OF "NOTICE OF PROPOSED CONSTRUCTION" FAA FORM 7460-1 MUST BE ACCOMPLISHED NOT EARLIER THAN 18 MONTHS AND NOT LATER THAN 120 DAYS PRIOR TO CONSTRUCTION.

E-FILE AT <https://oeaaa.faa.gov/oeaaa/external/portal.jsp>.

DESIGN AND STRUCTURAL

Description of the proposed project:

The proposed project will create 1.53 miles of shared-use path in the southeastern section of the City of Marietta along Franklin Gateway and Delk Road (SR 280) ending on Bentley Drive. The proposed project is to create a shared use path along Franklin Gateway to SR 280 (Delk Road (SR 280)) and along Delk Road (SR 280) then crossing over I-75 via pedestrian bridge. The shared use path continues to Rottenwood Creek and along Rottenwood Creek until it rejoins Delk Road (SR 280), where it crosses Rottenwood Creek via pedestrian bridge. The shared use path continues east on Delk Road (SR 280) until it terminates at Bentley Road. ADA accessible ramps, crosswalks at intersections, landscaping, lighting, seating areas, drainage improvements and pedestrian enhancements will be included in this project.

Major Structures

Structure	Existing	Proposed
Pedestrian Bridge south of Delk Road (SR 280) Bridge/North of Windy Hill Bridge	N/A	The proposed pedestrian bridge will be approximately 400 feet in length and 15 feet in width passing under the Northwest Corridor and spanning the entire width of I-75 and returning to ground in the planted buffer area between the sound wall barrier and the Motel 6 parking lot. The proposed bridge section is to be a 3-span bridge over the I-75 North and South bound lanes. The bridge is proposed from trail centerline station approx. 44+50 to 48+50. (See Concept Plan Typical Section in Attachments)
Pedestrian Bridge crossing Rottenwood Creek adjacent to Delk Road (SR 280) and west of Stratford Ridge Apartment Homes	N/A	The proposed pedestrian bridge is a single span bridge of approximately 85 feet in length and 12 feet in width spanning Rottenwood Creek where Rottenwood Creek passes under Delk Road (SR 280). (See Typical Section in Attachments). The bridge is proposed from trail centerline station approx. 73+25 to 74+10.
Retaining Wall, A-1	N/A	The proposed retaining wall will be a poured-in-place concrete retaining wall along Delk Road (SR 280) holding back the slope allowing the required area for the 10-foot shared use path. The wall is proposed from trail centerline station approx. 75+10 to 77+54.
Sound Wall east side of I-75	Standard paneled sound wall on east side of I-75 running south from Delk Road (SR 280) to Windy Hill.	1-2 segments of 1 panels of the existing sound wall barrier could possibly be removed to allow the pedestrian bridge to return to grade within the planted buffer area between the sound wall barrier and the Motel 6 parking lot. The sound wall occurs at approximate centerline station 48+50.

Accelerated Bridge Construction (ABC) techniques anticipated: No Yes

During Preliminary Engineering, when there are more detailed decisions about the bridge structure, there may be places to evaluate opportunities to utilize principles and techniques of the Accelerated Bridge Construction (ABC).

Mainline Design Features:

Franklin Gateway	Functional Classification: Local Road and Street		
Feature	Existing	*Policy	Proposed
Typical Section:			
- Number of Lanes	4		4
- Lane Width(s) (-ft)	11-ft	10-12-ft	11-ft
- Border Area Width (-ft)	5-15 ft	10-16-ft	5-15 ft
- Cross Slope (%)	2%	2%	2%
- Sidewalks (-ft)	Varies 4 – 8 ft concrete	5-ft	Varies 7 – 10 ft concrete
- Auxiliary Lanes (#lanes/-ft width)	3 lanes / 10-ft each		3 lanes / 10-ft each
- Bike Accommodations	N/A	4 – 5 ft	7-10 ft
- Posted Speed (mph)	35		35
- Design Speed (mph)	35	35	35
Maximum Superelevation Rate (%)	Normal Crown	4%	Normal Crown
- Maximum Grade (%)	3%	8%	3%
Design Vehicle	WB-67		WB-67
Check Vehicle	OSOW		OSOW
Pavement Type	HMA		HMA

*According to current GDOT Design Policy if applicable

Delk Road (SR 280)	Functional Classification: Minor Arterial		
Feature	Existing	*Policy	Proposed
Typical Section:			
- Number of Lanes	6		6
- Lane Width(s) (-ft)	11-ft	11-12 ft	11-ft
- Median Width (-ft) & Type	17-ft raised	24-ft raised	17-ft raised **
- Border Area Width (-ft)	10-40-ft	10-16-ft	10-40-ft
- Cross Slope (%)	2%	2%	2%
- Sidewalks (-ft) (0-5 ft	5 ft	5-10-ft concrete
- Auxiliary Lanes (#lanes/-ft width)	4 lanes / 12-ft each		4 lanes / 12-ft each
- Bike Accommodations	N/A	4-5-ft	10-ft
- Posted Speed (mph)	55		55
- Design Speed (mph)	55	55	55
Maximum Superelevation Rate (%)	4%	4%	4%
Maximum Grade (%)	8%	8%	8%
Access Control	Permitted	Permitted	Permitted
Design Vehicle	WB-67		WB-67
Check Vehicle	OSOW		OSOW
Pavement Type	HMA		HMA

*According to current GDOT Design Policy if applicable

** Existing 17-ft median is not being touched.

Rottenwood Creek Natural Area Trail	Functional Classification: Shared-Use Path		
Feature	Existing	*Policy	Proposed
Typical Section:			
- Cross Slope (%)	N/A	2%	2%
- Trails (-ft)	N/A	10 ft	10-ft concrete
- Bike Accommodations	N/A	4 – 5 ft	10-ft
Maximum Grade (%)	N/A	5%	5% **

*Requirements shown are per PROWAG standards

** Maximum grade on new location; grade to match existing paralleling roadway in border area. ~OB

Design Exceptions/Design Variances to FHWA or GDOT Controlling Criteria anticipated:

FHWA or GDOT Controlling Criteria	No	Undetermined	Yes	DE or DV	Approval Date (if applicable)
1. Design Speed	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
2. Design Loading Structural Capacity	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
3. Stopping Sight Distance	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
4. Horizontal Curve Radius	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
5. Maximum Grade	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
6. Vertical Clearance	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
7. Superelevation Rate	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
8. Lane Width	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
9. Cross Slope	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		
10. Shoulder Width	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>		

Design Variances to GDOT Standard Criteria anticipated:

GDOT Standard Criteria	No	Undetermined	Yes	Approval Date (if applicable)
1. Access Control	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
2. Shoulder Width	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
3. Intersection Sight Distance	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
4. Intersection Skew Angle	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
5. Tangent Lengths on Reverse Curves	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
6. Lateral Offset to Obstruction	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
7. Rumble Strips	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
8. Safety Edge	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
9. Median Usage	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
10. Roundabout Illumination Levels	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
11. Complete Streets Warrants	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
12. ADA Requirements in PROWAG	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
13. GDOT Construction Standards	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
14. GDOT Drainage Manual	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

Median Usage-The existing median will be evaluated during preliminary engineering on Delk Road locations and size of pedestrian landings and/or refuge area. The existing median width does not meet minimum GDOT

standard and any proposed use of the median would not increase the size any further. Concepts for the reconfiguration of crosswalk and median can be found in Attachment 11.

Shoulder Width- Will be verified during preliminary engineering.

VE Study anticipated: No Yes Completed:

Lighting Required: No Yes

The proposed lighting will include pedestrian lighting along the pathway. Franklin Gateway corridor already has both pedestrian and vehicular lighting. The proposed pedestrian lighting will begin on the south side of Delk Road (SR 280) at Franklin Gateway and continue along the trail to the termini at Delk Road (SR 280) and Bentley Road. The intent will be to utilize the City of Marietta pedestrian light standard at a spacing to be determined during preliminary engineering. The Lighting Commitment letter from the City of Marietta is included in Attachment 11.

Off-site Detours Anticipated: No Undetermined Yes

Transportation Management Plan [TMP] Required: No Yes
If Yes: Project classified as: Non-Significant Significant
TMP Components Anticipated: TTC TO PI

Significant TMP measures are associated with the construction of the pedestrian bridge over I-75 and how it may impact Interstate 75.

The rest of the shared-use pathway project will be classified as non-significant and require only TTC.

INTERCHANGES AND INTERSECTIONS

Interchanges/Major Intersections:

1. Franklin Gateway & Delk Road (SR 280)
Intersection signalized with mast arms with 9 total lanes (3 east-bound, 3 west-bound, 3 auxiliary turn). The shared use path will cross on the west side and on the south side.
2. Delk Road (SR 280) & Bentley Road
Intersection signalized with span wires. There are 6 total lanes (2 east-bound, 2 west-bound, 2 auxiliary turn lanes). The shared use path will not cross this intersection.
3. Proposed Pedestrian Bridge over I-75
There are 6 lanes going both north and 6 lanes going south. There are 2 lanes on the south bound on-ramp and 1 northbound lane on the off-ramp.

Intersection Control Evaluation (ICE) Required: No Yes

Roundabout Concept Validation Required: No Yes Completed

UTILITY AND PROPERTY

Railroad Involvement: Proposed project is not within a 500-ft proximity to a railroad.

Utility Involvements:
Atlanta Gas Light - Gas

AT&T-Telephone
 Comcast - Cable
 Cobb County Water
 Cobb County Sewer
 Cobb County Dept. of Transportation – Telecom
 Cobb County EMC
 Fiberlight LLC – Telecom
 Georgia Power – Electrical
 Century Link – Cable
 Marietta B&W– Electrical & Water
 SRTA –Northwest Corridor
 Zayo-Fiber

SUE Required: No Yes Undetermined
 SUE level A is recommended

Public Interest Determination Policy and Procedure recommended: No Yes

The proposed pedestrian bridge west of I-75 will consider and locate accordingly to the SRTA utility near the Northwest Corridor tollway. During preliminary engineering detailed consideration will be given to opportunities and constraints that utility creates.

Right-of-Way (ROW): Existing width: 80-300 ft. Proposed width: 80-300 ft.
 Franklin Gateway and Delk Road (SR 280) are the Right of Way widths listed above while the section east of I-75 to Delk Road (SR 280) will follow Rottenwood Creek in an existing sanitary easement that is 20-30 feet in width.

Required Right-of-Way anticipated: None Yes Undetermined
 Easements anticipated: None Temporary Permanent * Utility Other
 * *Permanent easements include the right to place utilities.*

Anticipated total number of impacted parcels:	17	
Displacements anticipated:	Businesses:	0
	Residences:	0
	Other:	0
Total Displacements:	0	

Location and Design approval: Not Required Required

Impacts to USACE property anticipated: No Yes Undetermined

The shared-use path will go through a portion of a floodplain for Rottenwood Creek East of 75 and south of Delk Road in the existing sanitary sewer easement.

ENVIRONMENTAL & PERMITS

Anticipated Environmental Document: *NEPA ~ CE*

Level of Environmental Analysis:

- The environmental considerations noted below are based on preliminary desktop or screening level environmental analysis and are subject to revision after the completion of resource identification, delineation, and agency concurrence.
- The environmental considerations noted below are based on the completion of resource identification, delineation, and agency concurrence.

MS4 Permit Compliance – Is the project located in a MS4 area? No Yes
If yes, is the GDOT MS4 Permit anticipated to apply to all or part of this project? No Yes

Is Non-MS4 water quality mitigation anticipated? No Yes

Environmental Permits/Variations/Commitments/Coordination anticipated:

Permit/Variance/Commitment/ Coordination Anticipated	No	Yes	Remarks
1. U.S. Coast Guard Permit	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
2. Forest Service/NPS	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
3. CWA Section 404 Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>	State waters east of I-75
4. Tennessee Valley Authority Permit	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
5. USACE Real Estate Outgrant	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
6. Buffer Variance	<input type="checkbox"/>	<input checked="" type="checkbox"/>	25' Stream Buffer Encroachment
7. Coastal Zone Management Coordination	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
8. NPDES	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
9. FEMA	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Floodplain east of I-75
10. Cemetery Permit	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
11. Other Permits	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
12. Other Commitments	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
13. Other Coordination	<input type="checkbox"/>	<input checked="" type="checkbox"/>	GDOT, SRTA, Northwest Corridor

During Preliminary Engineering phase, it will be determined if the trail impacts any Stream Buffers. If this is the case, at that time, a Stream Buffer Variance will be requested.

The project concept commits to follow all NPDES standards and guidelines to reduce or eliminate negative impacts to water quality from the construction of this project.

During Preliminary Engineering, the design team will coordinate pedestrian bridge design with the Northwest Corridor Project to ensure no impacts to the operation of the tollway with both GDOT and SRTA. FHWA coordination will also occur to follow all federal regulations along I-75.

Is a PAR required? No Yes Completed *Date*

Environmental Comments and Information:

NEPA/GEPA: No Section 4(f) properties were immediately identified in the desktop survey. The level of NEPA documentation is currently unknown.

Ecology: In addition to Rottenwood Creek, the National Wetland Inventory (NWI) map identified potential wetlands along the floodplain of Rottenwood Creek east of Interstate 75 (I-75) and south of Delk Road (SR 280). These resources were not delineated. The USFWS IPaC list identifies five species of concern within Cobb County, including the federally threatened Cherokee darter (*Etheostoma scotti*), little amphianthus (*Amphianthus pusillus*), northern long-eared bat (*Myotis septentrionalis*), and white fringeless orchid (*Platanthera integrilabia*) and the

federally endangered dwarf sumac (*Rhus michauxii*). The proposed project occurs within the Upper Chattahoochee River watershed [Hydrologic Unit Code (HUC) 03130001]. Surveying and delineation would be required prior to construction.

History: No National Register-listed historic resource was identified along the survey corridor. No historic resources were identified in the GNAHRGIS database. One historic resource was identified along the corridor based on information obtained from the Cobb County tax assessor's records. This resource is located at 2360 Delk Road (SR 280) and was constructed in 1968. Because eligibility determinations have not been made, this resource would need to be evaluated as a newly identified resource. Additional surveying will be required prior to project construction.

Archeology: Five previously recorded archaeological sites are located within a 1-kilometer radius of the proposed corridor. There are no previously recorded sites located within the currently proposed project area. Additional surveying will be required prior to construction.

Air Quality:

Is the project located in an Ozone Non-attainment area? No Yes
Is a Carbon Monoxide hotspot analysis required? No Yes

The project is a shared-use path that will not change the roadway. The project extents are along the west side of Franklin Gateway (3-4 lanes) down to Delk Road (SR 280, 9 lanes) and crossing on the west and south side of the signalized intersection on mast arms. The path will then go along the south side of Delk Road (SR 280) in a wide R/W area until it intersects with I-75 (6 lanes in each direction, 2 southbound on-ramps, 1 north-bound off ramp). The pathway will then head south to Rottenwood Creek in a natural area and travel north back to Delk Road (not SR 280 east of I-75). Delk Road east of I-75 is 2 lanes in each direction with 2 auxiliary lanes. The project will extend to Bentley Road and terminate on the SW corner of the intersection. Bentley Road is signalized, but no crossings are planned at this time for the shared-use path. The proposed open to traffic year is approximately 2028 for the pedestrian bridge.

Due to project type, it is unlikely that any Mobile Source Air Toxic (MSAT) or Carbon Monoxide (CO) analysis would be required.

Noise Effects: Due to the nature of the project (trail construction) there are no anticipated impacts to noise levels. As a result, a Type I Noise Assessment is anticipated.

The possible removal of portions of individual sound panels from a sound wall section will be studied for possible effects.

Public Involvement: A PIOH was held on November 21, 2019. A total of two comments were received out of seven total attendees, both in favor of the project.

Major stakeholders: Cumberland CID, Cobb County, Gateway Marietta CID, City of Marietta, SRTA, and GDOT

CONSTRUCTION

Issues potentially affecting constructability/construction schedule:

The construction of the pedestrian bridge over I-75-

Construction of the piles and bridge supports on the edges and in the median of I-75 and the setting of the pedestrian bridge may require special hours of construction to reduce impacts on traffic flow. Special traffic control may be required to provide transitions of traffic lanes. Traffic control plans will be submitted to GDOT and FHWA for approval during the design process.

Construction period of pathway near Rottenwood Creek-

Construction of the pedestrian corridor should be completed during summer months when the creek levels are lower.

Early Completion Incentives recommended for consideration: No Yes

COORDINATION, ACTIVITIES, RESPONSIBILITIES, AND COSTS

Initial Concept Team Meeting: The Initial Concept Team Meeting was held October 17, 2019. Several design intent documents were sent out to the attendees to illustrate the intended proposed enhancements. These documents and meeting minutes can be found in Attachment 10.

Concept Team Meeting: The Concept Team Meeting occurred February 26th, 2020. The concept team meeting presentation, attendees list, meeting minutes and comments can be found in Attachment 10.

Other coordination to date: Local engagement on September 19 and 27 of 2019.

Project Activity	Party Responsible for Performing Task(s)
Concept Development	Consultant-Stantec
Design	Consultant/Project is currently Scoping-Only
Right-of-Way Acquisition	Local
Utility Coordination (Preconstruction)	Consultant
Utility Relocation (Construction)	Utility Owners
Letting to Contract	Local
Construction Supervision	Local/Consultant
Providing Material Pits	Contractor
Providing Detours	Contractor
Environmental Studies, Documents, & Permits	Consultant
Environmental Mitigation	Local
Construction Inspection & Materials Testing	Local/Consultant/GDOT

≒Local refers to City of Marietta

Project Cost Estimate Summary and Funding Responsibilities**:						
	PE Activities		ROW	Reimbursable Utilities	CST*	Total Cost
	PE Funding	Section 404 Mitigation				
Date of Estimate:	12/17/2019	5/12/20	6/18/20	1/27/20	11/10/20	
Funded By:	ARC	Local/TBD	Local/TBD	Local/TBD	Local/TBD	
Programmed Cost:	\$598,369		\$0.00	\$0.00	\$0.00	\$598,369
Estimated Cost:	\$1,250,000	\$51,900	\$1,600,000	\$50,000	\$11,673,261	\$14,625,161
Total Cost Difference:						\$14,026,792

~OB

*CST Cost includes: Construction, Engineering and Inspection, Contingencies and Liquid AC Cost Adjustment.
 -Cost estimates prepared by the design team.

ALTERNATIVES DISCUSSION

Alternative selection:

Preferred Alternative: 10-foot-wide shared-use path from end of Phase I Rottenwood Creek Trail to Bentley Road via Franklin Gateway, Delk Road, and a pedestrian bridge over I-75.

Estimated Property Impacts:	17	Estimated Total Cost:	\$14,625,161
Estimated ROW Cost:	\$1,600,000	Estimated CST Time:	18 months

This alternative was selected due to the opportunity to span a pedestrian bridge over I-75 and move trail users from one side of I-75 to the other. Other alternatives were either not physically or financially feasible. The trail will provide the connections and mobility opportunity across the city by continuing the Phase I section. The area is continuing to grow, and this pedestrian/mobility network as a gateway and catalyst along the Franklin Gateway and Delk Road Corridor.

*Estimates by design team.

No-Build Alternative: Phase I of the Rottenwood Creek Trail coming from near downtown Marietta will terminate at Franklin Gateway. Users of the trail would have to figure out the mobility and connectivity opportunities from that point. The desired east-west connection from Marietta towards the Chattahoochee River would not connect to the terminus of the Bob Callan trail that leads users to Truist Stadium and ultimately to the Chattahoochee River. There will also not be an accessible pedestrian connection over I-75 that connect this growing area of Franklin Gateway with Delk Road East of I-75 and eastern portions of Cobb County.

Estimated Property Impacts:	0	Estimated Total Cost:	\$0.00
Estimated ROW Cost:	\$0.00	Estimated CST Time:	0 months

While there is a not a dollar cost to a no-build option, there is an opportunity that would be lost. The Gateway Marietta CID and Delk Road (SR 280) Corridor serve as a gateway to Marietta. The trail has the opportunity to act as an economic driver for the burgeoning Gateway Marietta CID as well as the Delk Road (SR 280) Corridor that is ripe for redevelopment and re-invigoration. There would also not be the desired accessible pedestrian connection between the two sides of Delk Road (SR 280) and Franklin Gateway.

*Estimates by the design team

Alternative 1: The trail path would pick up on Rottenwood Creek Trail route for Phase I and continue along Rottenwood Creek Trail until it gets to I-75. At that point it would pass under I-75 from new construction of an I-75 Bridge over Rottenwood Creek (currently box culverts). The bridge would lift the elevation of I-75 in that area. The trail would continue east of I-75 along Rottenwood Creek until it hit the Delk Road (SR 280) bridge in which it would pass underneath with a raised boardwalk. The trail would then exit from the creek south of the bridge and move to Bentley Road.

Estimated Property Impacts:	25	Estimated Total Cost:	\$30,000,000
Estimated ROW Cost:	\$1,000,000*	Estimated CST Time:	24 months

Rationale: The environmental, structural, and roadway impacts to this alternative would be exceedingly beyond the scope of the city's intent of the trail network. The Northwest Corridor would have to be protected in place and subsurface studies would be required to not damage either the Northwest Corridor or I-75. The traffic impacts to I-75 with the construction of a bridge would be excessive for the purpose of putting a trail underneath it. This option would be difficult to explain to the public in a cost vs. benefit scenario.

*Estimates by design team.

Alternative 2: The trail path would pick up on Rottenwood Creek Trail Phase I at Franklin Gateway and follow Franklin Gateway south to Delk Road (SR 280). The path would take Delk all the way to Bentley Road and terminate east of I-75.			
Estimated Property Impacts:	30	Estimated Total Cost:	\$3,000,000*
Estimated ROW Cost:	\$2,500,000*	Estimated CST Time:	12 months
Rationale: The issue with this alternative is that I-75 has a number of free-flow on-ramps and off-ramps. Coordination with GDOT Traffic Operations revealed that trail crossings over these entrances and exits of I-75 would create operational challenges for I-75 and therefore would not be a viable operational alternative without extensive roadway, signal, and timing improvements and additional traffic analysis.			

*Estimates by design team.

Comments:

1. With the project's vicinity and interaction with Rottenwood Creek, additional green infrastructure opportunities will be evaluated to maintain or improve the water quality of the creek.
2. The City of Marietta is committed to maintaining proposed lighting on this corridor after construction as per the signed lighting commitment letter in Attachment 11.
3. The City of Marietta will maintain the landscaping along the corridor post-construction including any landscaping in the median or shoulder.
4. Any revision to the existing median will include Type 7 curb and contain in-between landscaping that meets all codes.
5. Coordination between SRTA, GDOT, and FHWA regarding I-75 and the Northwest Corridor will occur throughout the design process.
6. Coordination will occur between Cobb Transit to determine viability of retaining or moving transit stops along the corridor.
7. The design of the trail will ensure that Emergency vehicles will have access with minimal conflict.
8. The database used for the development of this project included available GIS data, some survey, and aerial photography. Impacts to property were considered using this information.
9. Traffic engineering design will review and analyze existing signal timing to determine adequacy for pedestrian and bicycle traffic during preliminary engineering
10. Clear zones will be met on Delk Road as shown in Attachment 1

LIST OF ATTACHMENTS/SUPPORTING DATA

1. Concept Layout
2. Typical sections
3. Detailed Cost Estimates:
 - a. Construction including Engineering and Inspection and Contingencies
 - b. Revisions to Programmed Costs forms, & Liquid AC Cost Adjustment forms
 - c. Right-of-Way
 - d. Environmental Mitigation
 - e. Utilities
4. Concept Utility Report
5. Crash summaries and diagrams
6. Capacity analysis summary
7. S I & A Report(s)
 - a. Soils summary and description
 - b. Detailed Geotechnical study including borings will included in preliminary engineering study
8. MS4 Concept Report Summary
 - a. MS4 Concept Report Summary
 - b. MS4 Drainage Area Layout
9. Minutes of Concept meetings
 - a. Initial Concept Meeting held 10/17/2019
 - b. Concept Meeting Minutes, held 2/26/2020
 - i. Draft Concept Report comments with responses are attached
10. Minutes of any meetings that shows support or objection to the concept
 - a. PIOH November 21, 2019
 - b. Public Outreach during Marietta concert series
 - i. September 19, 2019 mid-day
 - ii. September 27, 2019 evening
11. Other items referred to in the body of the report
 - a. Complete Streets Warrants
 - i. Bicycle
 - ii. Pedestrian
 - iii. Transit
 - b. Potential bridge concept graphics
 - c. Phase I ESA Acceptance Memo
 - d. Median concepts at Delk and Franklin Gateway Intersection
 - e. Lighting Commitment Letter
 - f. Example Mowing and Maintenance Agreement for City of Marietta
 - g. Project Justification Statement Approval

APPROVALS

Concur: 	1/8/21
_____ Director of Engineering	_____ Date
Approve: _____	1/8/2021
_____ Chief Engineer	_____ Date

Attachment 1 Concept Layout

CHARACTER AREA 1

Urban environment characterized by close association between pedestrians, moving traffic, slope, utility, and crossing challenges.

FRANKLIN RD.

DELK RD.

I-75

CHARACTER AREA 3

Existing underutilized drainage easement. Presents opportunity for wooded greenspace respite within an urban desert.

CHARACTER AREA 4

Low income housing offers a opportunity for community interaction, connectivity, equitable planning for all.

BENTLEY RD.

CHARACTER AREA 2

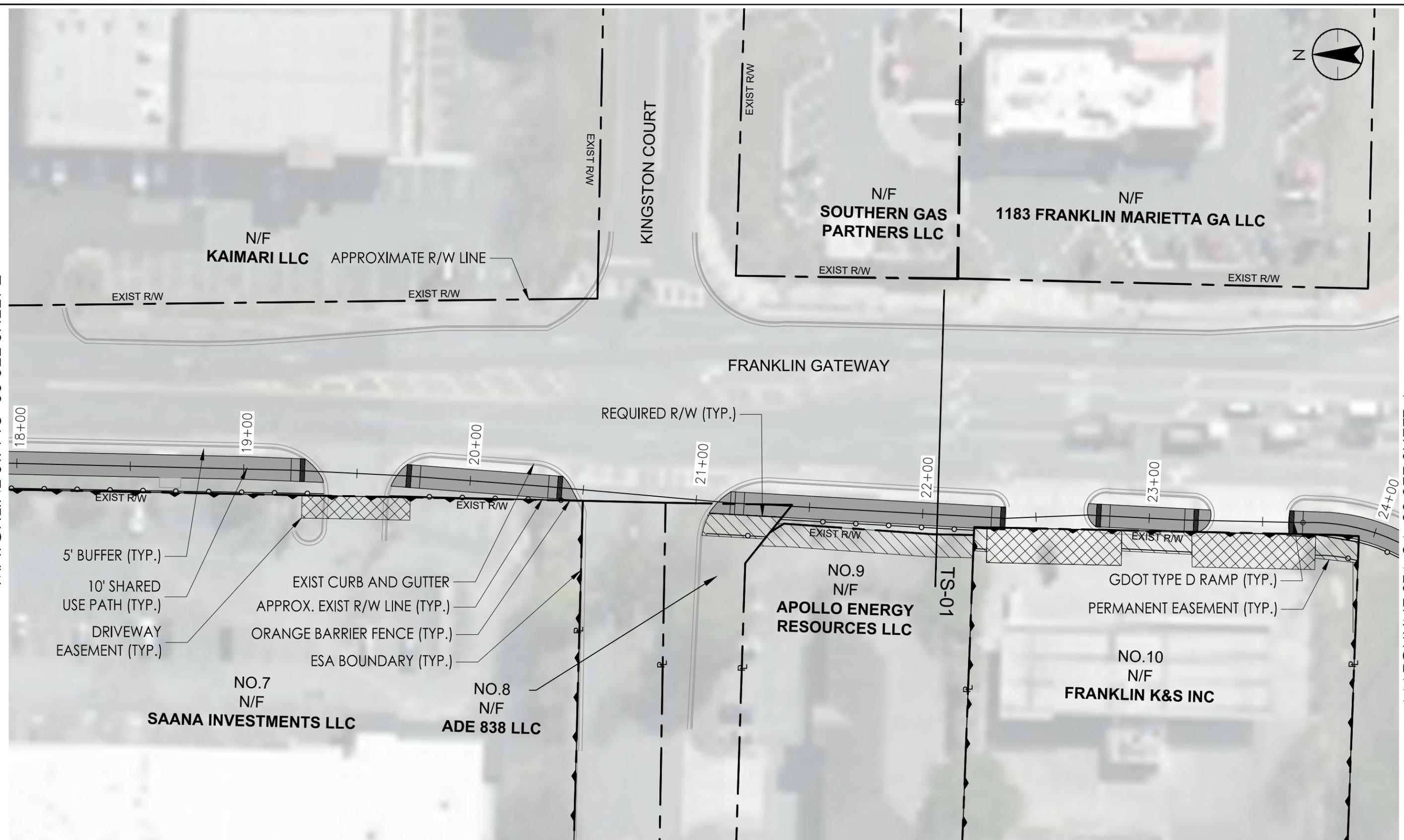
Transition zone between raised parking lot grade and proposed pedestrian bridge over I-75.



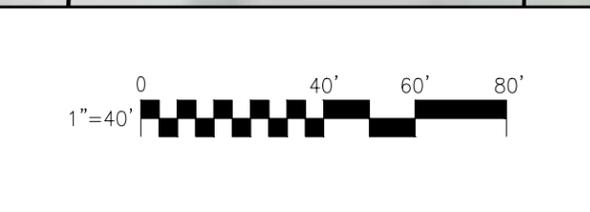


MATCHLINE STA 18+00 SEE SHEET 2

MATCHLINE STA 24+00 SEE SHEET 4



PROPOSED SHARED-USE PATH		PERMANENT EASEMENT / EASEMENT FOR CONSTRUCTION & MAINTENANCE OF SLOPES	
EXISTING PROPERTY LINE		EASEMENT FOR CONSTR OF DRIVES	
EXISTING R/W LINE		ENVIRONMENTALLY SENSITIVE AREA	
REQUIRED R/W LINE			
ORANGE BARRIER FENCE			



REVISION DATES

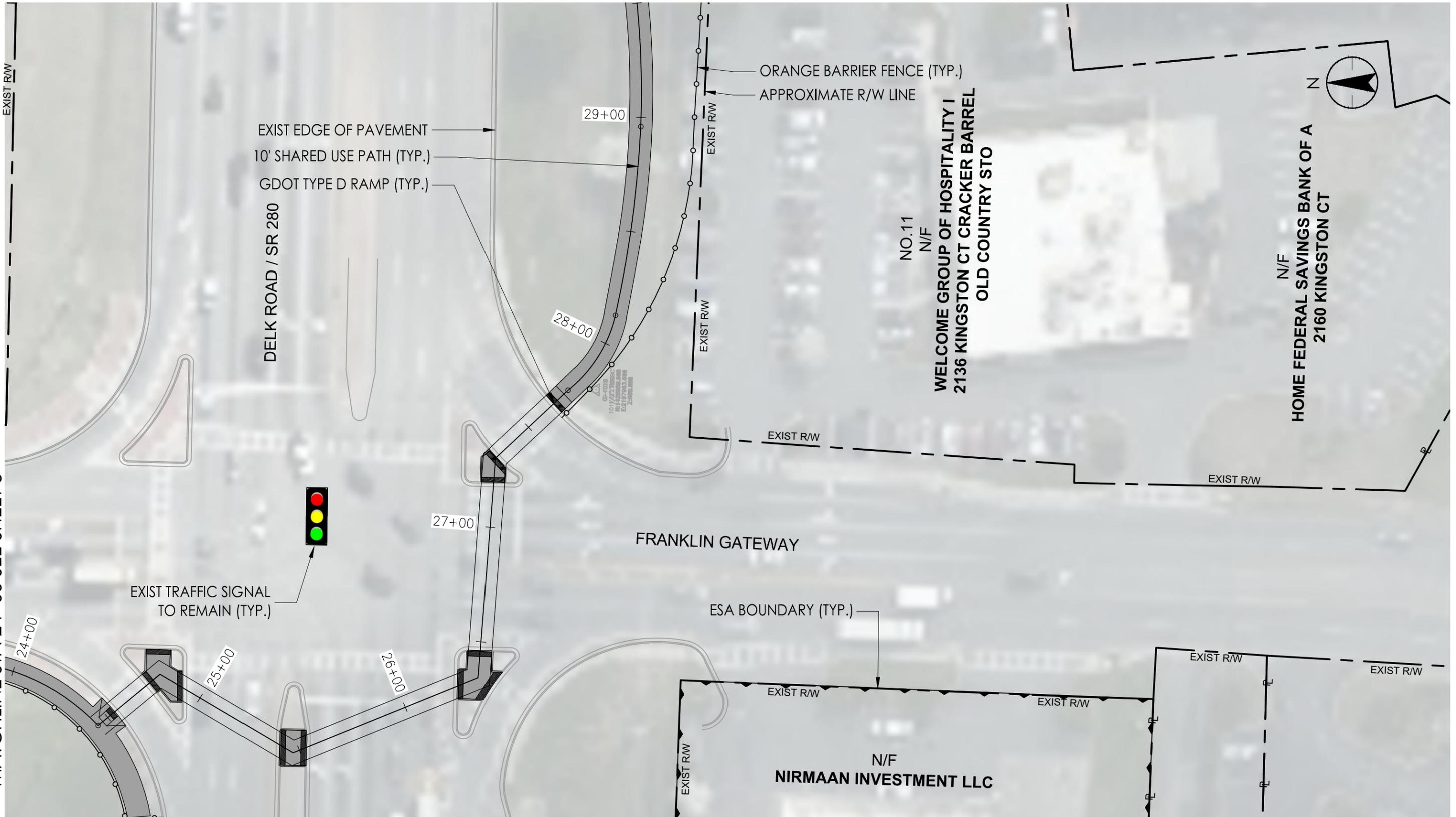
COBB COUNTY, GEORGIA

CONCEPT PLAN - ROTTENWOOD CREEK TRAIL
 PHASE II - SCOPING ONLY - P.I. NUMBER 0015049

DRAWING No.
 3 OF 13

MATCHLINE STA 29+50 SEE SHEET 5

MATCHLINE STA 24+00 SEE SHEET 3



PROPOSED SHARED-USE PATH	
EXISTING PROPERTY LINE	
EXISTING R/W LINE	
REQUIRED R/W LINE	
ORANGE BARRIER FENCE	

PERMANENT EASEMENT / EASEMENT FOR CONSTRUCTION & MAINTENANCE OF SLOPES	
EASEMENT FOR CONSTR OF DRIVES	
ENVIRONMENTALLY SENSITIVE AREA	

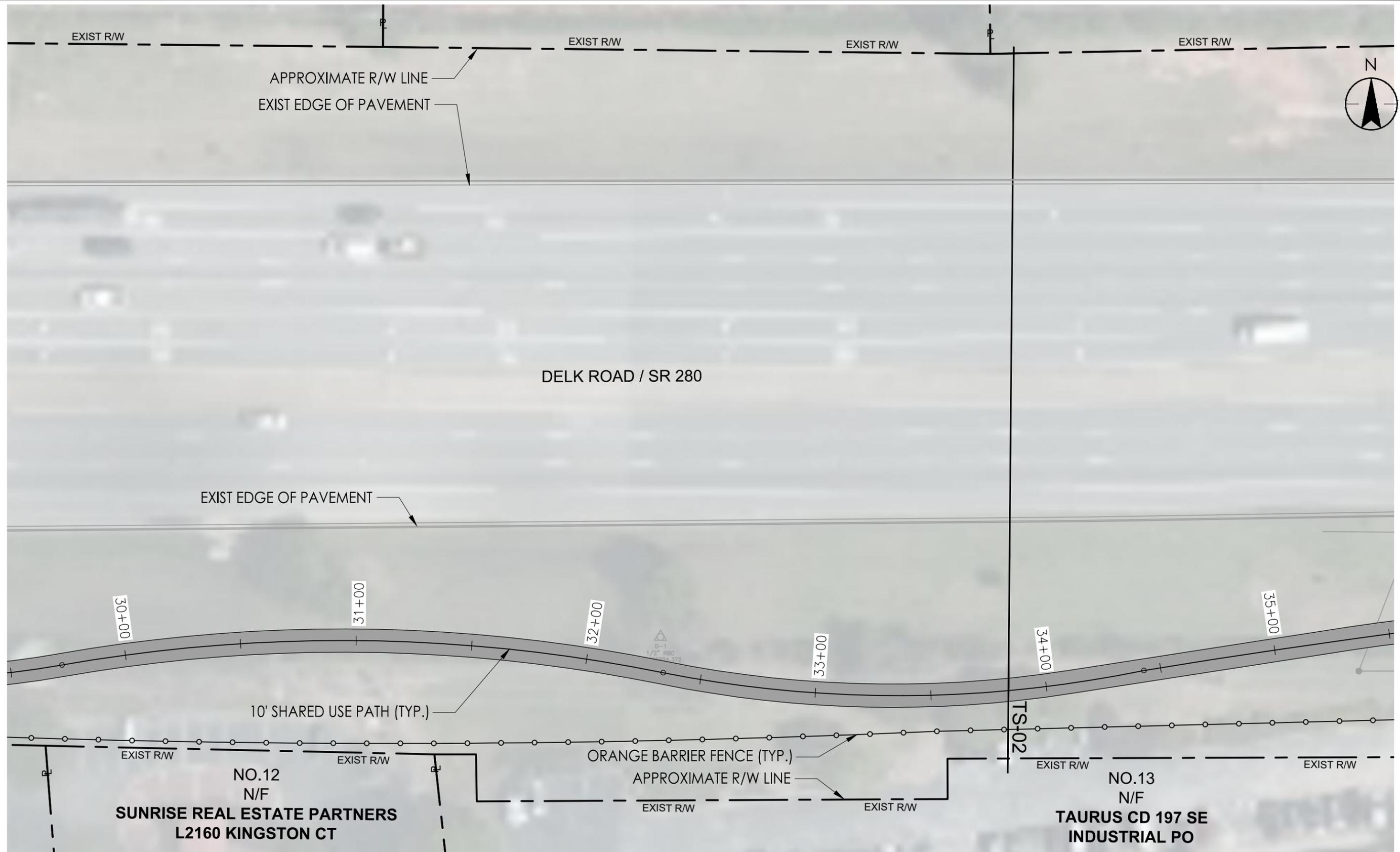


REVISION DATES

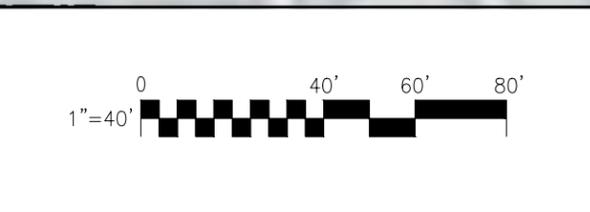
COBB COUNTY, GEORGIA	
CONCEPT PLAN - ROTTENWOOD CREEK TRAIL PHASE II - SCOPING ONLY - P.I. NUMBER 0015049	
DRAWING No.	4 OF 13

MATCHLINE STA 29+50 SEE SHEET 4

MATCHLINE STA 35+50 SEE SHEET 6



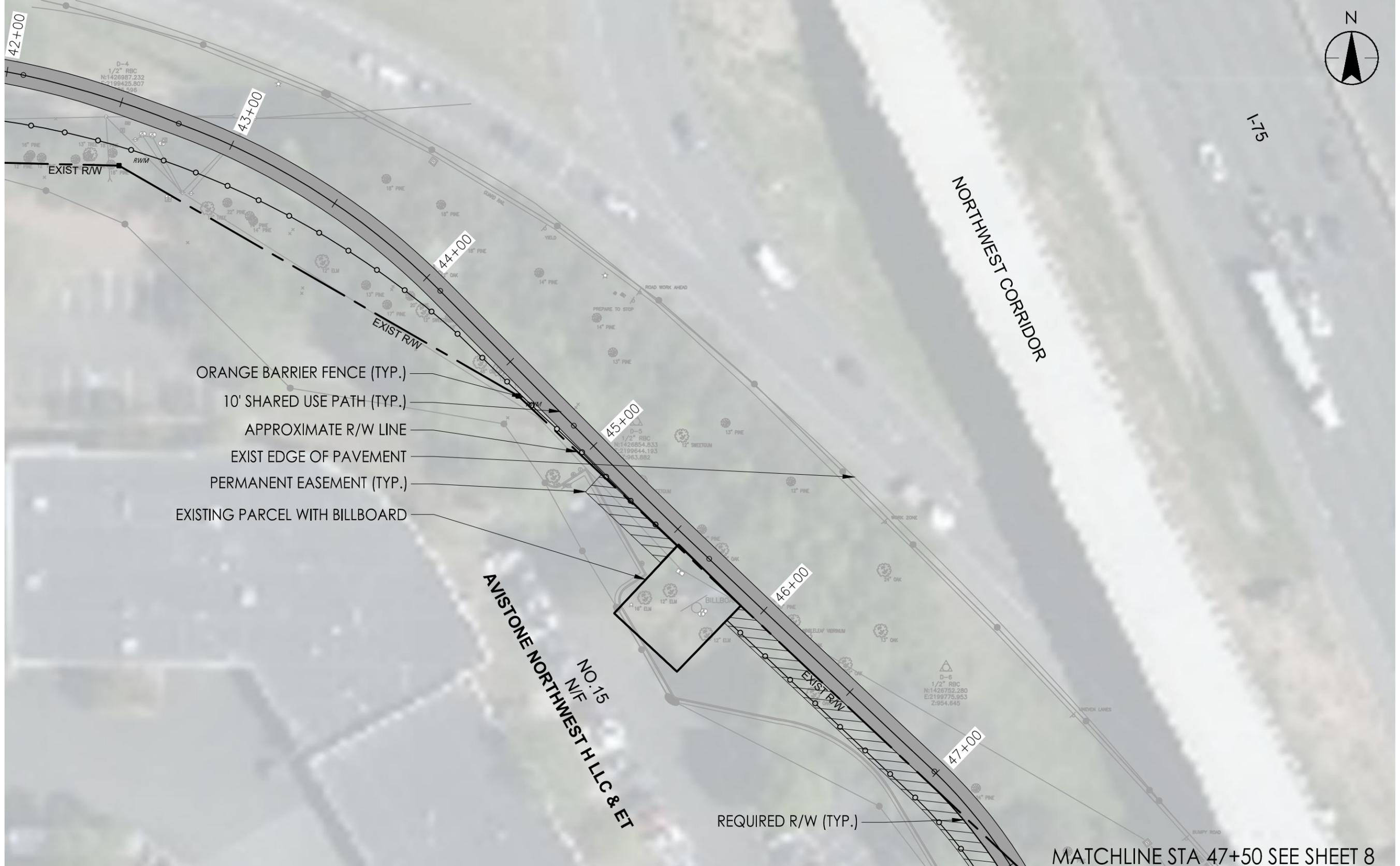
PROPOSED SHARED-USE PATH		PERMANENT EASEMENT / EASEMENT FOR CONSTRUCTION & MAINTENANCE OF SLOPES	
EXISTING PROPERTY LINE		EASEMENT FOR CONSTR OF DRIVES	
EXISTING R/W LINE		ENVIRONMENTALLY SENSITIVE AREA	
REQUIRED R/W LINE			
ORANGE BARRIER FENCE			



REVISION DATES

COBB COUNTY, GEORGIA	
CONCEPT PLAN - ROTTENWOOD CREEK TRAIL PHASE II - SCOPING ONLY - P.I. NUMBER 0015049	
DRAWING No.	5 OF 13

MATCHLINE STA 42+00 SEE SHEET 6



- ORANGE BARRIER FENCE (TYP.)
- 10' SHARED USE PATH (TYP.)
- APPROXIMATE R/W LINE
- EXIST EDGE OF PAVEMENT
- PERMANENT EASEMENT (TYP.)
- EXISTING PARCEL WITH BILLBOARD

AVISTONE NORTHWEST H LLC & ET
NO. 15
N/F

MATCHLINE STA 47+50 SEE SHEET 8

PROPOSED SHARED-USE PATH	
EXISTING PROPERTY LINE	
EXISTING R/W LINE	
REQUIRED R/W LINE	
ORANGE BARRIER FENCE	

PERMANENT EASEMENT / EASEMENT FOR CONSTRUCTION & MAINTENANCE OF SLOPES	
EASEMENT FOR CONSTR OF DRIVES	
ENVIRONMENTALLY SENSITIVE AREA	

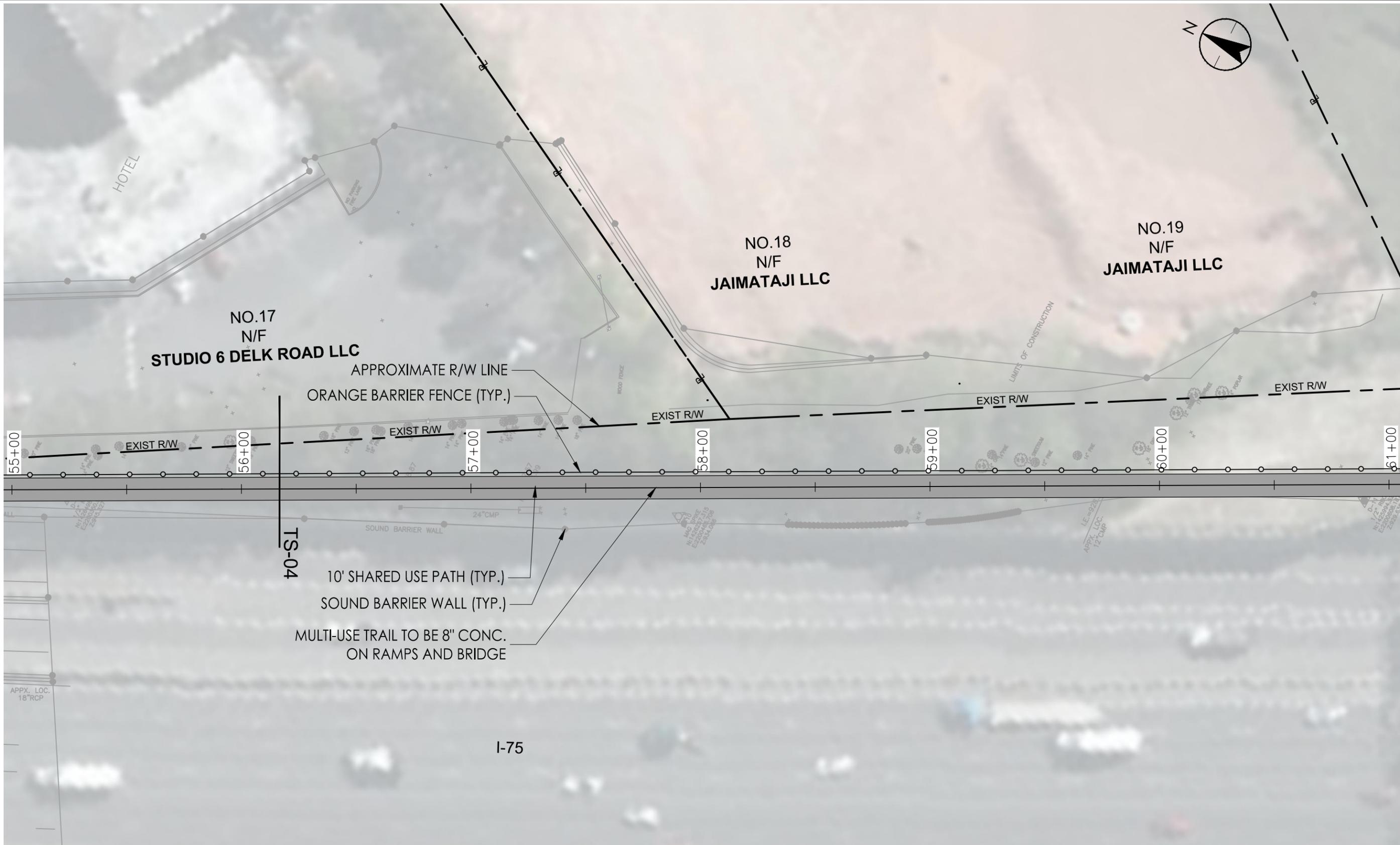


REVISION DATES

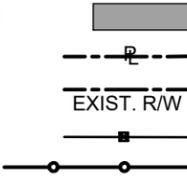
COBB COUNTY, GEORGIA	
CONCEPT PLAN - ROTTENWOOD CREEK TRAIL PHASE II - SCOPING ONLY - P.I. NUMBER 0015049	
DRAWING No.	7 OF 13

MATCHLINE STA 55+00 SEE SHEET 8

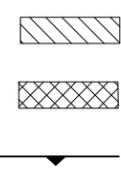
MATCHLINE STA 61+00 SEE SHEET 10



PROPOSED SHARED-USE PATH
 EXISTING PROPERTY LINE
 EXISTING R/W LINE
 REQUIRED R/W LINE
 ORANGE BARRIER FENCE



PERMANENT EASEMENT / EASEMENT FOR CONSTRUCTION & MAINTENANCE OF SLOPES
 EASEMENT FOR CONSTR OF DRIVES
 ENVIRONMENTALLY SENSITIVE AREA



REVISION DATES

COBB COUNTY, GEORGIA

CONCEPT PLAN - ROTTENWOOD CREEK TRAIL
 PHASE II - SCOPING ONLY - P.I. NUMBER 0015049

DRAWING No.
 9 OF 13

MATCHLINE STA 68+00 SEE SHEET 10



NO.21
N/F
MESQUITE CREEK DEVELOPMENT INC

NO.22
N/F
HARDEES FOOD
SYSTEMS

NO.23
N/F
WILENSKY
LARRY E

APPROXIMATE PROPERTY LINE
ORANGE BARRIER FENCE (TYP.)
PERMANENT EASEMENT (TYP.)

REQUIRED R/W (TYP.)

68+89

69+00

70+00

71+00

72+00

73+00

74+00

COBB COUNTY SEWER EASEMENT

10' SHARED
USE PATH (TYP.)
APPROXIMATE
PROPERTY LINE

ROTTENWOOD CREEK

20'

MATCHLINE STA 74+50 SEE SHEET 12

- PROPOSED SHARED-USE PATH
- EXISTING PROPERTY LINE
- EXISTING R/W LINE
- REQUIRED R/W LINE
- ORANGE BARRIER FENCE

- PERMANENT EASEMENT / EASEMENT FOR CONSTRUCTION & MAINTENANCE OF SLOPES
- EASEMENT FOR CONSTR OF DRIVES
- ENVIRONMENTALLY SENSITIVE AREA



REVISION DATES

COBB COUNTY, GEORGIA

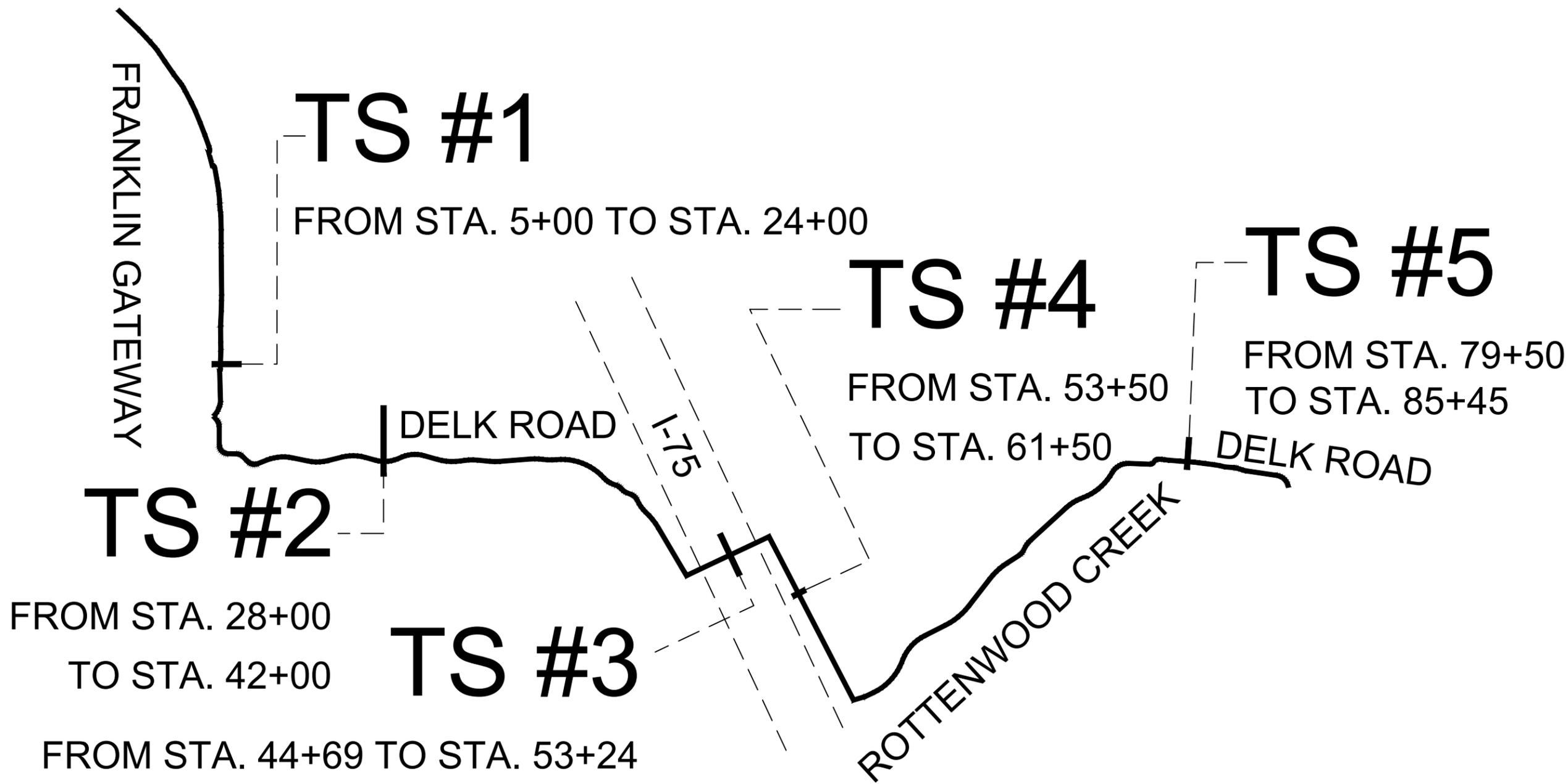
CONCEPT PLAN - ROTTENWOOD CREEK TRAIL
PHASE II - SCOPING ONLY - P.I. NUMBER 0015049

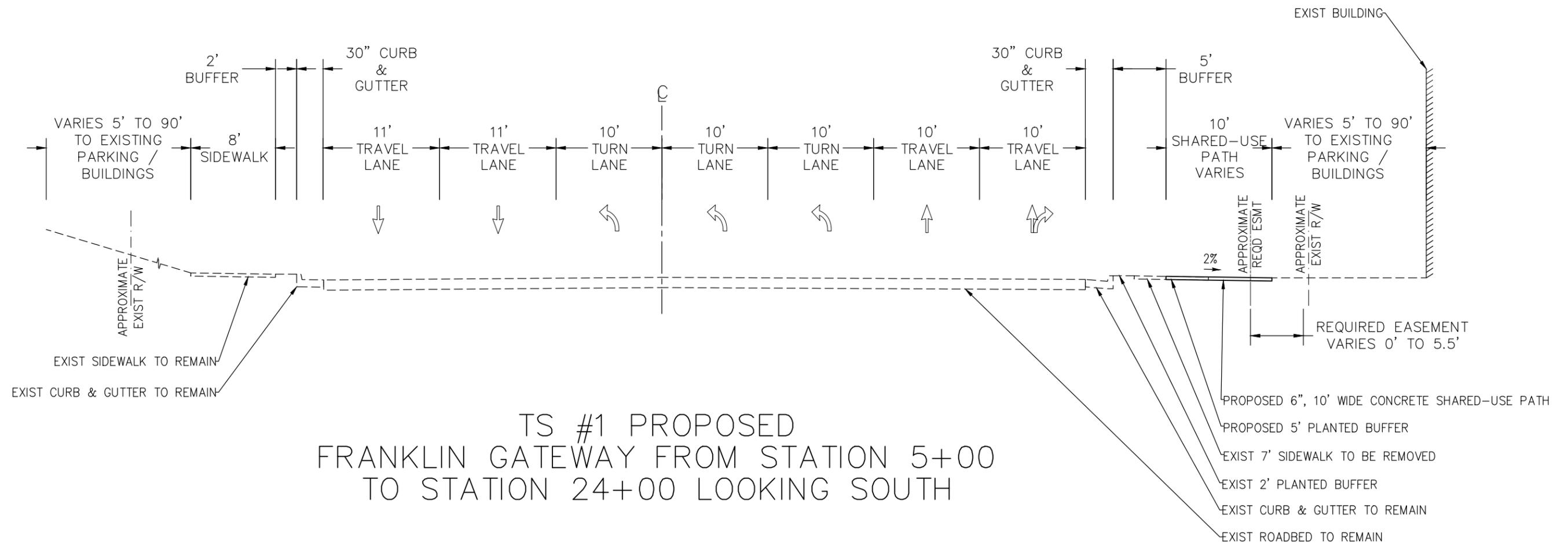
DRAWING No.
11 OF 13

Attachment 2

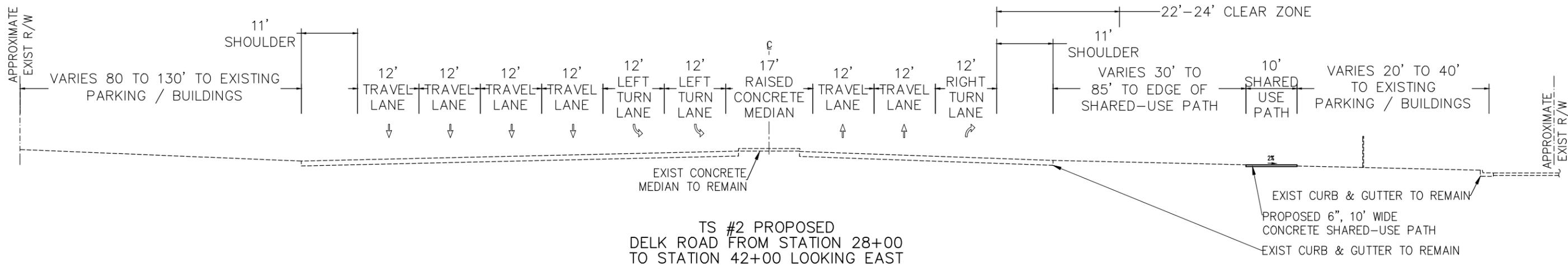
Typical Sections

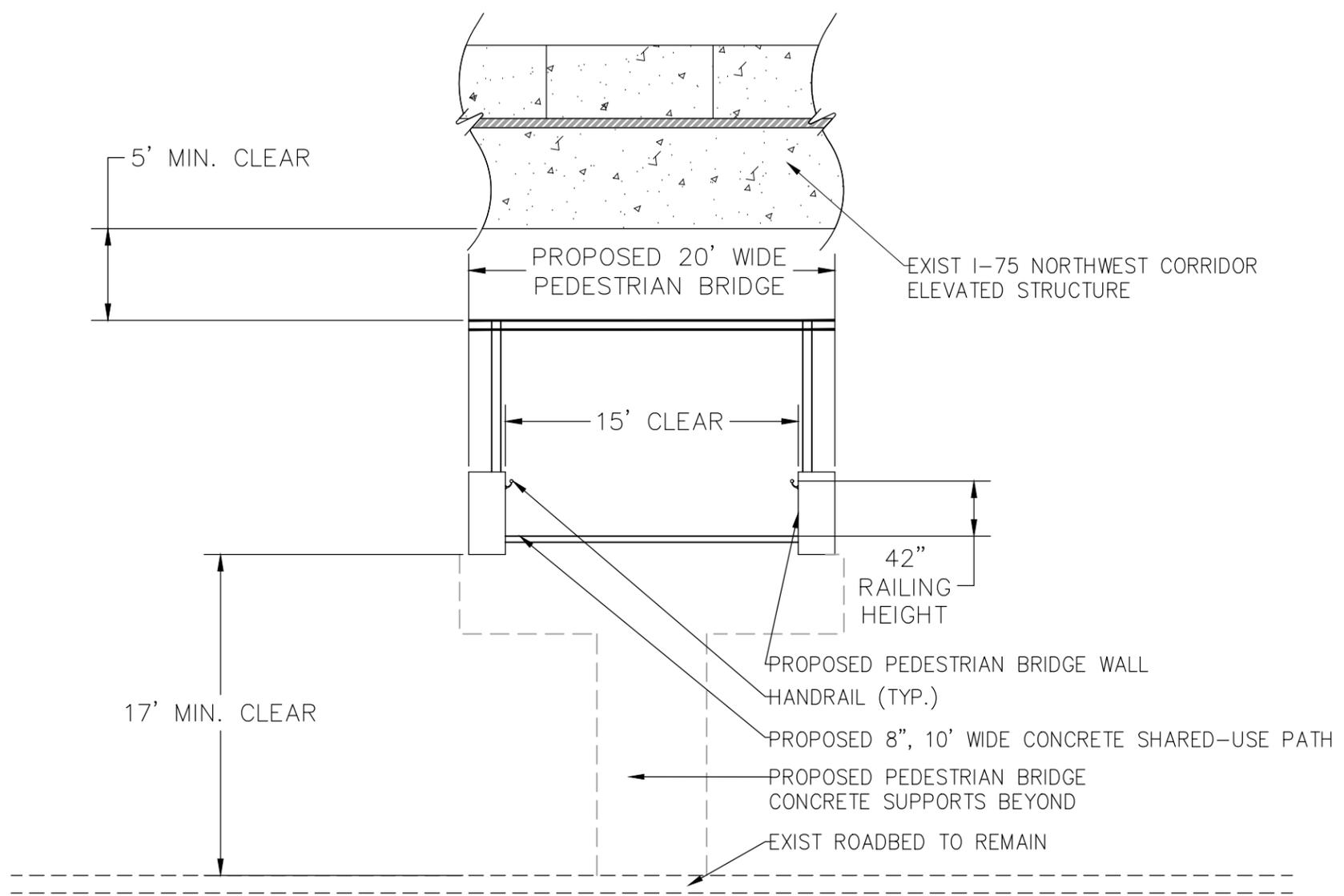
- a. Context Map
- b. Typical Sections
- c. Offset typicals for trees and light poles



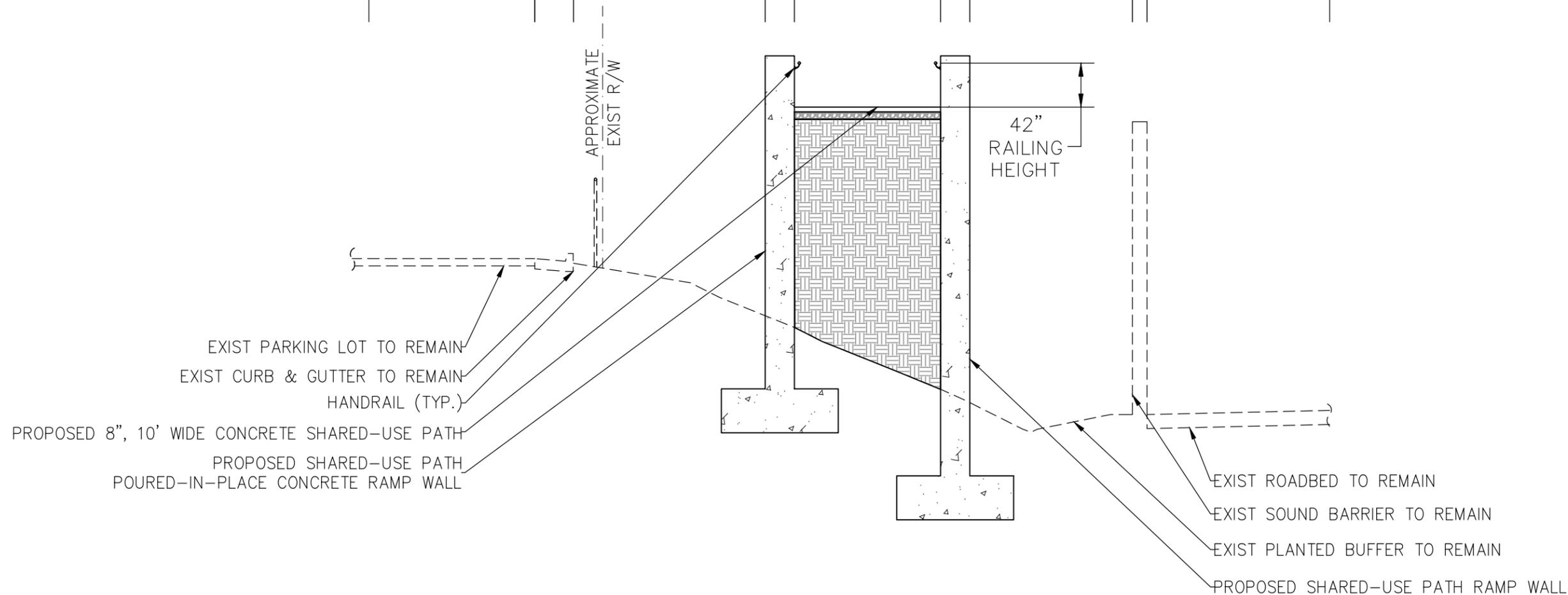
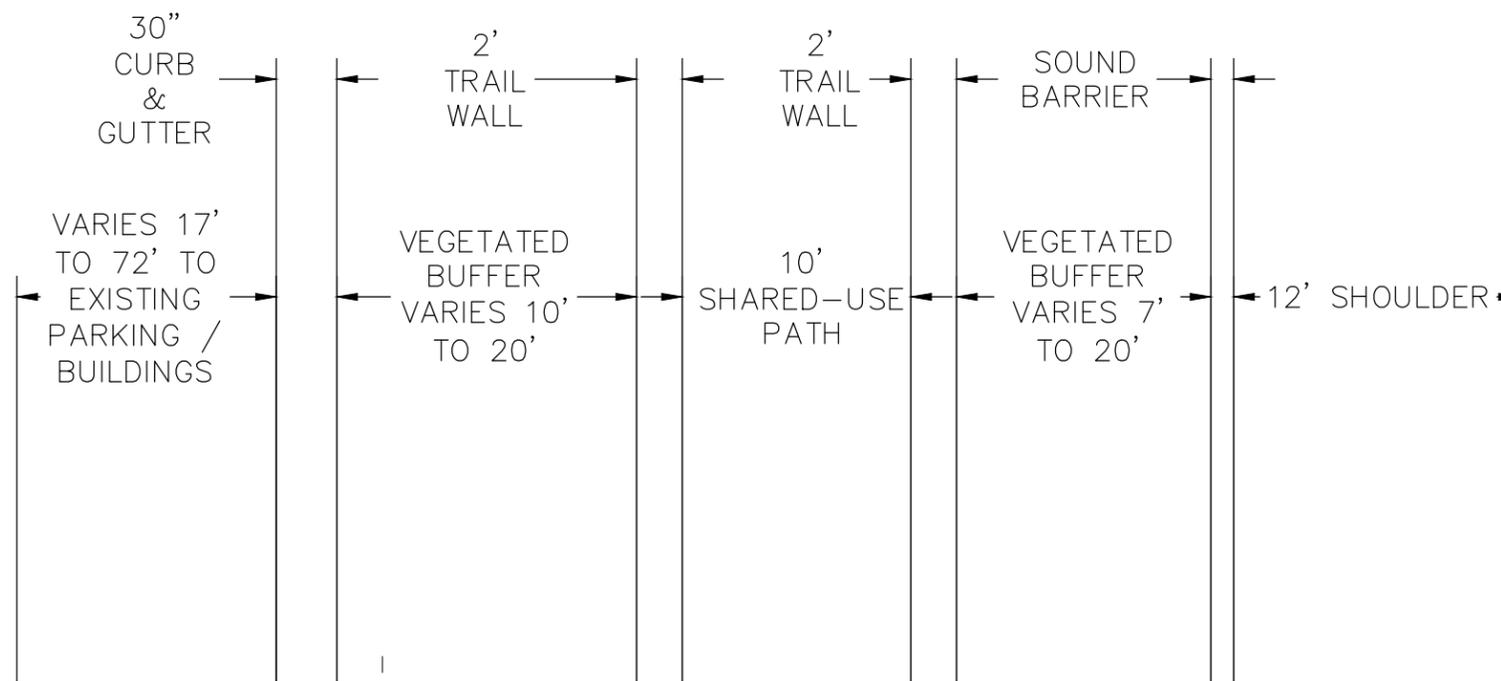


TS #1 PROPOSED
 FRANKLIN GATEWAY FROM STATION 5+00
 TO STATION 24+00 LOOKING SOUTH

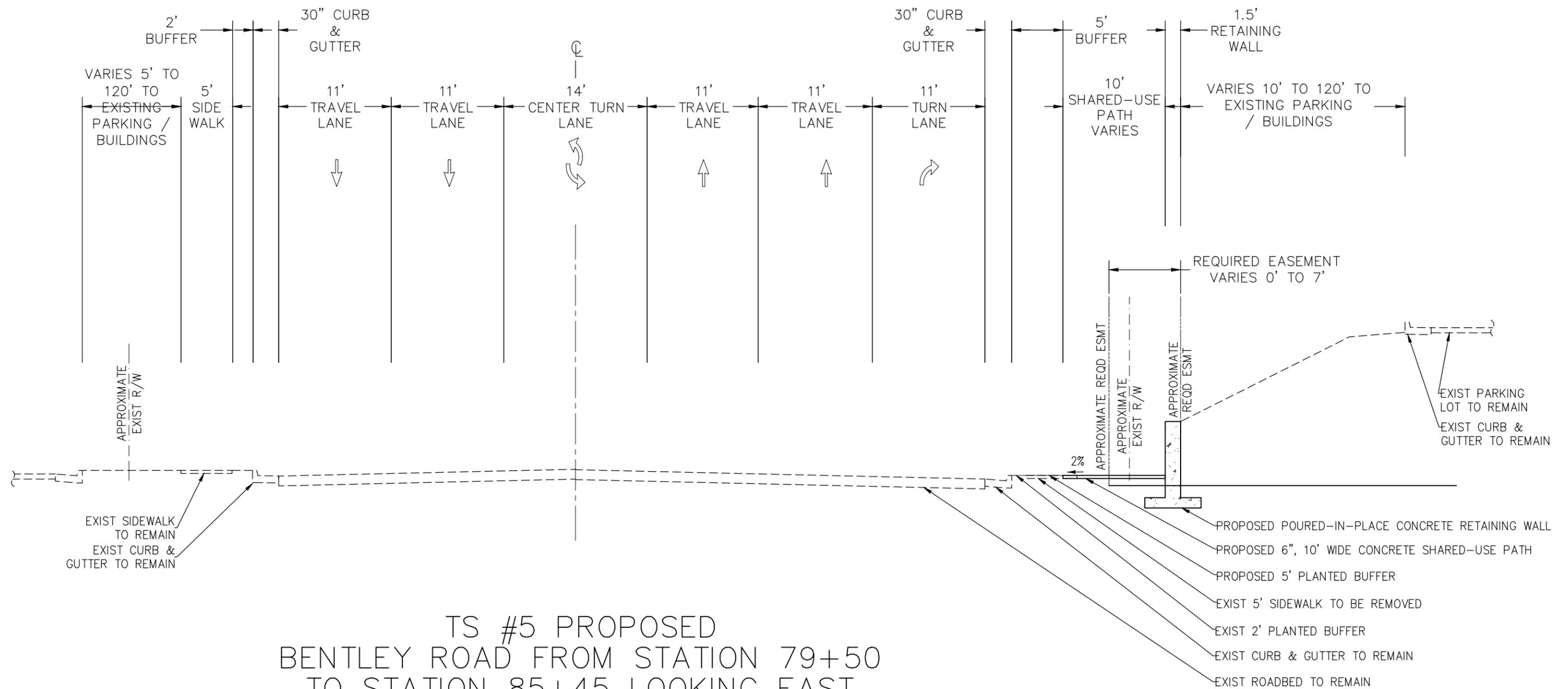


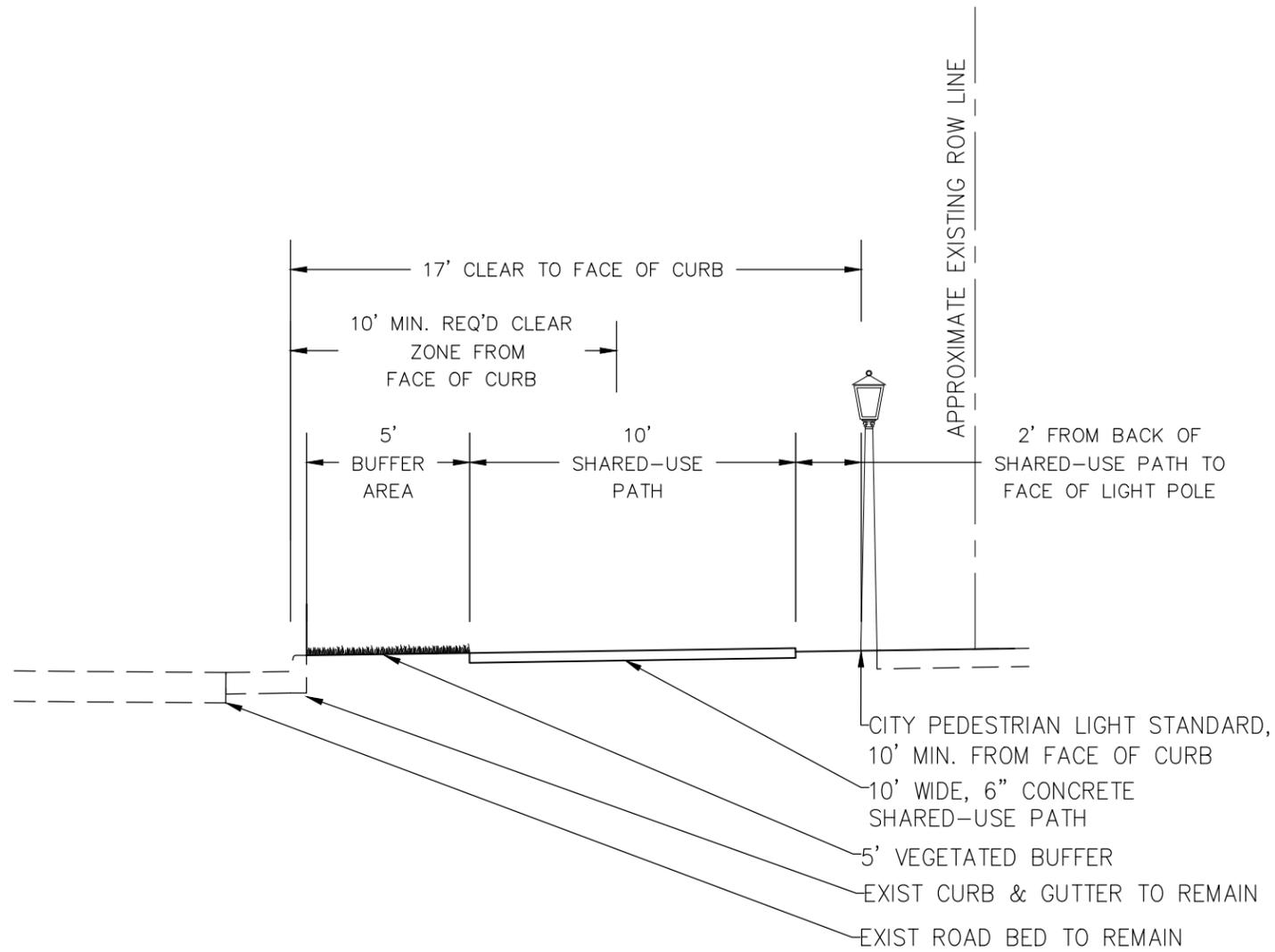


TS #3 PROPOSED
 FROM STATION 44+69
 TO STATION 53+24 LOOKING EAST



TS #4 PROPOSED
 FROM STATION 53+50 TO
 STATION 61+50 LOOKING SOUTHEAST





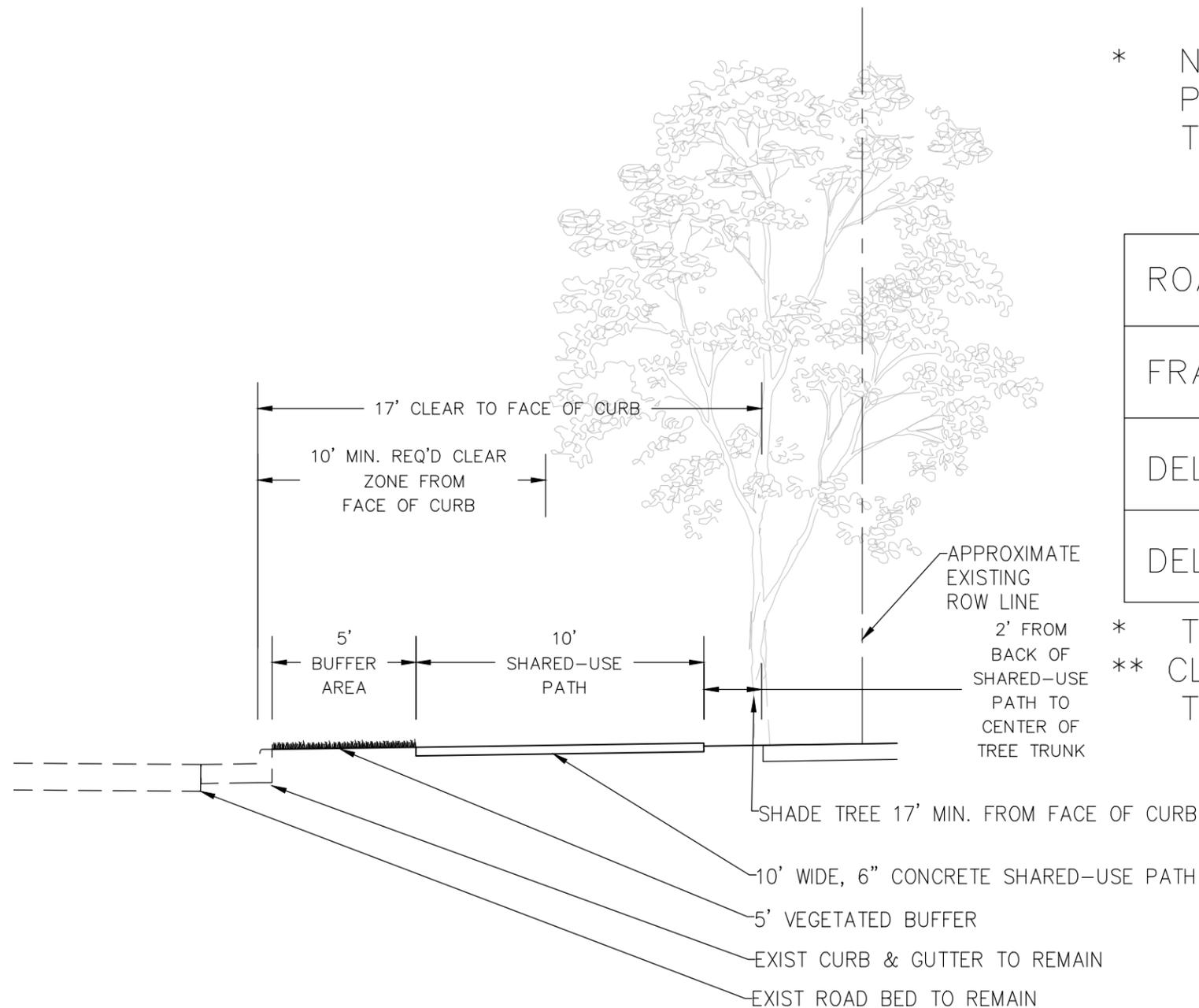
* NOTE: ALL TREES AND LIGHTS WILL BE PLACED OUTSIDE REQUIRED CLEAR ZONE PER TABLE 1-A

ROAD NAME	SPEED LIMIT	REQUIRED CLEAR ZONE DISTANCE**
FRANKLIN GATEWAY ROAD	35 MPH	14-16 FT
DELK ROAD (WEST OF I-75)	55 MPH	22-24 FT
DELK ROAD (EAST OF I-75)	45 MPH	20-22 FT

* TABLE 1-A

** CLEAR ZONE DISTANCE IS MEASURED FROM THE EDGE OF THE TRAVEL LANE

TYPICAL LIGHTING DETAIL



* NOTE: ALL TREES AND LIGHTS WILL BE PLACED OUTSIDE REQUIRED CLEAR ZONE PER TABLE 1-A

ROAD NAME	SPEED LIMIT	REQUIRED CLEAR ZONE DISTANCE**
FRANKLIN GATEWAY ROAD	35 MPH	14-16 FT
DELK ROAD (WEST OF I-75)	55 MPH	22-24 FT
DELK ROAD (EAST OF I-75)	45 MPH	20-22 FT

* TABLE 1-A
 ** CLEAR ZONE DISTANCE IS MEASURED FROM THE EDGE OF THE TRAVEL LANE

TYPICAL TREE DETAIL

Attachment 3

Detailed Cost Estimates:

- a. Construction including Engineering and Inspection and Contingencies
- b. Revisions to Programmed Costs forms, & Liquid AC Cost Adjustment forms
- c. Right-of-Way
- d. Preliminary Section 404 Mitigation Cost Estimate

Cost Estimate Summary Report

Report 042 v7

Cost Estimate Name:	0015049	Improvement Type:	ENHN
Description:	Rottenwood Creek Multi-use Trail Phase II Scoping Only Cost Estimate	Work Type:	ENHN
Spec Book:	13	Estimated By:	Stephen Hopper
Unit System:	English		

Cost Estimate Total: \$11,012,510.18

Base Cost Estimate

Total Cost Estimate Items:	\$11,012,510.18
Total Typical Section Pricing:	\$0.00
Total Ad Hoc Pricing:	\$0.00

Cost Category Breakdown

Total Construction Costs:	\$11,012,510.18
Total Non Construction Costs:	\$0.00
Total Bid Costs:	\$11,012,510.18

Cost Estimate Budget Class Report

Report v1

Cost Estimate: 0015049 - 0015049

Cost Estimation Phase: 2-DE

Cost Estimate Item Total: \$11,012,510.18

Cost Estimate Budget Class Report - Estimate Level Details

Budget Class	Amount	Assignment Level
	\$11,012,510.18	Cost Estimate

Cost Estimate Budget Class Report

Report v1

Cost Estimate Budget Class Report - Item Level Details

Budget Class	Line Number	Item	Item Description	Quantity	Unit	Amount
	5	163-0550	CONSTRUCT AND REMOVE INLET SEDIMENT TRAP	20.000	EA	\$4,549.89
	10	165-0010	MAINTENANCE OF TEMPORARY SILT FENCE, TP A	3,500.000	LF	\$1,659.32
	15	167-1500	WATER QUALITY INSPECTIONS	12.000	MO	\$8,019.95
	20	441-0016	DRIVEWAY CONCRETE, 6 IN TK	100.000	SY	\$755,000.00
	25	441-0106	CONC SIDEWALK, 6 IN	7,550.000	SY	\$377,500.00
	30	210-0100	GRADING COMPLETE -	1.000	LS	\$400,000.00
	35	167-1000	WATER QUALITY MONITORING AND SAMPLING	3.000	EA	\$792.29
	40	150-1000	TRAFFIC CONTROL -	1.000	LS	\$150,000.00
	45	163-0240	MULCH	15.000	TN	\$6,773.28
	50	171-0010	TEMPORARY SILT FENCE, TYPE A	7,000.000	LF	\$21,000.00
	55	441-5002	CONCRETE HEADER CURB, 6 IN, TP 2	500.000	LF	\$10,410.34
	60	500-3107	CLASS A CONCRETE, RETAINING WALL	1,526.000	CY	\$768,316.58
	65	516-1100	ALUM HANDRAIL, STD 3626	1,000.000	LF	\$131,295.39
	70	534-1000	PEDESTRIAN OVERPASS BRIDGE, STA -	1.000	LS	\$150,000.00
	75	534-1000	PEDESTRIAN OVERPASS BRIDGE, STA -	1.000	LS	\$7,000,000.00
	80	550-1180	STORM DRAIN PIPE, 18 IN, H 1-10	500.000	LF	\$33,045.46
	85	636-1041	HIGHWAY SIGNS, TP 2 MATL, REFL SHEETING, TP 9	100.000	SF	\$3,220.79
	90	636-1045	HIGHWAY SIGNS, TP 2 MATL, REFL SHEETING, TP 11	100.000	SF	\$2,298.15

Cost Estimate Budget Class Report

Report v1

Cost Estimate Budget Class Report - Item Level Details

Budget Class	Line Number	Item	Item Description	Quantity	Unit	Amount
	95	643-8200	BARRIER FENCE (ORANGE), 4 FT	3,300.000	LF	\$6,482.85
	100	643-8300	ORNAMENTAL FENCE	2,000.000	LF	\$250,000.00
	105	647-6086	AUDIBLE PUSHBUTTON STATION WITH SIGN	3.000	EA	\$4,500.00
	110	668-2100	DROP INLET, GP 1	10.000	EA	\$26,992.93
	115	681-3600	LIGHTING STD, SPCL DESIGN	68.000	EA	\$323,000.00
	120	700-6001	GRASSING - COMPLETE	1.000	AC	\$20,000.00
	125	700-7000	AGRICULTURAL LIME	5.000	TN	\$658.35
	130	700-8000	FERTILIZER MIXED GRADE	5.000	TN	\$3,839.87
	135	700-8100	FERTILIZER NITROGEN CONTENT	50.000	LB	\$214.76
	140	702-0365	ILEX CORNUTA BURFORDI -	1,000.000	EA	\$45,000.00
	145	702-0559	LIRIOPE MUSCARI -	2,500.000	EA	\$15,000.00
	150	702-0905	QUERCUS PHELLOS -	60.000	EA	\$3,600.00
	155	708-1000	PLANT TOPSOIL	50.000	CY	\$6,250.00
	160	754-4000	WASTE RECEPTACLE UNIT	15.000	EA	\$22,500.00
	165	754-5000	BENCH	10.000	EA	\$25,000.00
	170	900-0039	BRICK PAVERS	2,800.000	SF	\$42,000.00
	175	900-0526	BOLLARDS	15.000	EA	\$11,250.00
	180	999-9500	GRANITE FACING	2,000.000	SF	\$60,000.00
	185	441-0108	CONC SIDEWALK, 8 IN	1,389.000	SY	\$84,126.05
	190	653-1804	THERMOPLASTIC SOLID TRAF STRIPE, 8 IN, WHITE	700.000	LF	\$2,159.70
	195	681-6200	LUMINAIRE, TYPE 2, LED	68.000	EA	\$78,095.42

Cost Estimate Budget Class Report

Report v1

Cost Estimate Budget Class Report - Item Level Details

Budget Class	Line Number	Item	Item Description	Quantity	Unit	Amount
	200	153-1100	FIELD ENGINEERS OFFICE TP 1	1.000	EA	\$50,000.00
	205	652-3501	SKIP TRAFFIC STRIPE, 5 IN, WHITE	1.520	GLM	\$633.34
	210	636-2070	GALV STEEL POSTS, TP 7	280.000	LF	\$2,325.47
	215	455-1000	FILTER FABRIC FOR EMBANKMENT STABILIZATION	5,000.000	SY	\$25,000.00
	220	999-0055	BIOSLOPE	1,000.000	LF	\$80,000.00

Interoffice Memo

FILE

PI NUMBER	0015049	PROJECT DESCRIPTION	The proposed project is to create a shared use path along Franklin Gateway to SR 280 (Delk Road (SR 280)) and along Delk Road (SR 280) crossing over I-75 via ped bridge terminating at Bentley Road at Delk road
OFFICE	Program Delivery		
DATE	Sunday, November 10, 2020 -OB		

From:

To: Erik Rohde, P.E., State Project Review Engineer
via email Mailbox: CostEstimatesandUpdates@dot.ga.gov

Subject: REVISIONS TO PROGRAMMED COSTS

Project Manager:	Olusola Adekonojo, GDOT PM
Management Let Date:	TBD-Scoping Only
Management Right of Way Date:	TBD-Scoping Only

Cost Estimate Review Iteration

Date of Submittal #1	
Date of Submittal #2	
Date of Submittal #3	

Summary of Programmed Costs and Proposed Revised Costs:

Estimate Type	Cost Estimate Amounts (T-Pro Without Inflation)	Last Estimate Date	Revised Cost Estimate
CONSTRUCTION	TBD	N/A	\$11,673,260.79
RIGHT OF WAY	TBD	N/A	\$1,600,000.00
UTILITIES	TBD	N/A	\$50,000.00

Explanation for Cost Change and Contingency Justification:

The project is for Scoping Only. Construction, right-of-way, and utility funds have not been allocated to this project yet. Estimates were completed by the design team.

Attachments:

Design Phase Leader Validation of Final QC/QA for Construction Cost Estimate Used In This Revision to Programmed Costs:

Consultant Company or GDOT Design Office:	Stantec
---	---------

Printed Name:	Stephen Hopper
---------------	----------------

Title:	Associate
--------	-----------

Signature:	
------------	---

Date:	11/10/2020
-------	------------

FOR PROJECTS WITH A LOCAL SPONSOR

If the project has a local sponsor, the project manager should ensure that the local authority completes the following validation indicating that it has reviewed the construction cost estimate and whether it is in concurrence with the construction costs presented.

- Please select the appropriate validation below upon review of the cost estimate:
- I acknowledge that I have reviewed the project construction cost estimate and concur with the costs presented.
 - I acknowledge that I have reviewed the project construction cost estimate but do not concur with the costs presented.

Please provide an explanation for non-concurrence.	
--	--

Local Authority Name and Title:	Marc Simmons, Transportation Engineer, City of Marietta
---------------------------------	---

Local Authority Signature:	
----------------------------	--

Cost Estimate Worksheet:

CONSTRUCTION COST ESTIMATE (Required base estimate entered from CES and should not include E&I). →										A	\$ 11,012,510.18
ENGINEERING AND INSPECTION (The default E&I percentage is 5.0%, but may be adjusted per project scope.) →										D	\$ 660,750.61
Construction Cost		E&I Percentage		E&I Cost							
B		C		D = B x C							
\$ 11,012,510.18		6%		\$ 660,750.61							
CONTINGENCY (Refer to the Risk and Contingencies Table included in GDOT Policy 3A-9 Cost Estimating Purpose) →										I	\$ -
Construction Cost		E&I Cost		Construction + E&I		Contingency Percentage		Contingency Cost			
E		F		G = E + F		H		I = G x H			
\$ 11,012,510.18		\$ 660,750.61		\$ 11,673,260.79		0%		\$ -			
ASPHALT FUEL PRICE ADJUSTMENT (Leave blank if not applicable) →										Q	\$ -
Date		Oct 2020		Current Asphalt Fuel Index Prices can be found at the link below: http://www.dot.ga.gov/PS/Materials/AsphaltFuelIndex							
Regular Unleaded		\$2.003/ GAL									
Diesel		\$2.439/ GAL									
Liquid AC		\$422.00/ TON									
Liquid AC		Tons	Percentage of Asphaltic Concrete	Tons of Asphaltic Concrete	Total Monthly Tonnage of Asphalt Cement (TMT)	Monthly Asphalt Cement Price month project let (APL)	Max. Cap	Monthly Asphalt Cement Price month placed (APM)	Price Adjustment (PA)		
	Description	J	K	L = J x K	M = Sum of Columns L, T & W	N	O	P = (N x O) + N	Q = [(P - N) / N] x M x N		
	Leveling				0.00 TN	\$422.00/ TON	60%	\$ 675.20	\$ -		
	Patching										
	9.5 mm SP										
	12.5 OGFC										
	12.5 PEM										
	12.5 mm SP										
	19 mm SP										
	25 mm SP										
Bituminous Tack Coat		Tack Coat	GL/TN	Tons							
	Description	R	S	T = R/S							
	Tack Coat		232.8234 GL/TN								
Bituminous Tack Coat (Surface Treatment)		SY	GL/SY	TN							
	Description	U	V	W = (U x V) / (232.8234 GL/TN)							
	Single Surface Treatment		0.20 GI/SY								
	Double Surface Treatment		0.44 GI/SY								
	Triple Surface Treatment		0.71 GI/SY								
CONSTRUCTION TOTAL COST →										X = A+D+I+Q	\$ 11,673,260.79
RIGHT OF WAY COST →										Y	\$ 1,600,000.00
UTILITIES COST (Provided by Utility Office) →										Z = Sum of Reimbursable Costs	\$ 50,000.00
Utility Owner		Reimbursable Cost		Utility Owner		Reimbursable Cost					
AT&T		\$ -									
Comcast		\$ -									
Marietta Water		\$ -									
Cobb County Water		\$ -									
Cobb County Sewer		\$ -									
Marietta Power		\$ -									
AGL		\$ -									
SRTA-NW Corridor		\$ 50,000.00									
Zaya-Fiber		\$ -									
Cobb EMC		\$ -									
CenturyLink		\$ -									
Georgia Power		\$ -									

Georgia Department of Transportation
Local Acquisition - Detailed ROW Cost Estimate Worksheet

Note: ROW Cost Estimate developed by Design Team

Date (MM/YYYY): June-20 Project: Rottenwood Creek Trail Phase II
 Revised: County: Cobb
 PI: 15049

Description: City of Marietta, Rottenwood Creek Trail

Parcels: 17 R/W Plan Date: 5/1/2020

FOR FUNDING ONLY

CONTRACT

Land and Improvements _____ \$1,293,651.19

Relocation _____ \$0.00

Demolition _____ \$0.00

SUB TOTAL (Reimbursable) _____ \$1,293,651.19

Valuation Services (Non-reimbursable) _____ \$35,625.00

Legal Services (Non-reimbursable) _____ \$123,975.00

SUB TOTAL (Non-reimbursable) _____ \$159,600.00

IN-HOUSE

Sponsor In-house _____ \$143,000.00

TOTAL ESTIMATED COSTS _____ \$1,596,251.19

TOTAL ESTIMATED COSTS (ROUNDED) _____ \$1,600,000.00

Preparation Credits	Hours	Signature

Gregory N. Malcolm, MAI

*CG#: 857

Gregory N. Malcolm
(DATE)

*CG#:

June 18, 2020

**CG required only if used for Negotiations*

Attachment(s): **Project Location Map; Subject/Comp Location Map; Comparable Sales Data**

May 12, 2020

PRELIMINARY SECTION 404 MITIGATION COST ESTIMATE
PI No. 0015049 Cobb County

Shared use path along Franklin Gateway to SR 280 (Delk Road (SR 280)) and along Delk Road (SR 280) crossing over I-75 via ped bridge, continuing to Rottenwood Creek and along Rottenwood Creek until it rejoins Delk Road.

While we do not anticipate any impacts on the stream bank of Rottenwood Creek, if there were any minor impingement, we would mitigate those impacts with a multitrophic planting approach. Any impacted areas would be revegetated with a combination of native trees, shrubs and grasses appropriate for the stream bank microclimate of the Rottenwood Creek.

Based on a worst-case scenario of 1,730 feet of permanent impact to perennial streams, the estimated mitigation required would be as follows:

At an estimated 30 dollars per impacted linear foot, the 1,730 linear feet of Rottenwood Creek bank in a worst-case scenario would yield a \$51,900 stream bank revegetation cost.

Attachment 4

Concept Utility Report

- a. Utility Cost Estimate Spreadsheet
- b. Concept Utility Report

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

INTERDEPARTMENT CORRESPONDENCE

FILE

Project No:	n/a	Office:	District 7 Utilities
County:	Cobb	Date:	January 27,2020
P.I.#:	0015049		

Description: *The proposed project is to create a shared use path along Franklin Gateway to SR 280 (Delk Road (SR 280)) and along Delk Road (SR 280) crossing over I-75 via ped bridge. The shared use path continues to Rottenwood Creek and along Rottenwood Creek until it rejoins Delk Road (SR 280). The shared use path continues down Delk Road (SR 280) until it hits Bentley Road and turns down Bentley Road for a short length and terminates. ADA accessible ramps, crosswalks at intersections, landscaping, lighting, seating areas and pedestrian enhancements will be included in this project.*

FROM Stantec

TO Olusola Adenkonojo, GDOT Project Manager

SUBJECT PRELIMINARY UTILITY COST ESTIMATE *NOTE: Utility Estimate developed by Design Team -OB

A review of utilities located on the above referenced project has been conducted with Concept Layout plans. Listed below is a breakdown of the anticipated reimbursable and non-reimbursable cost.

Utility Owner	Reimbursable	Non-Reimbursable	Estimate Based on
AT&T	\$0.00	\$22,375.00	Preliminary info from Utility
Comcast	\$0.00	\$20,000.00	Preliminary info from Utility
Marietta Water	\$0.00	\$35,750.00	Preliminary info from Utility
Cobb County Water	\$0.00	\$29,750.00	Preliminary info from Utility
Cobb County Sewer	\$0.00	\$1,750.00	Preliminary info from Utility
Cobb County DOT-Telecom	\$0.00	\$18,000.00	Preliminary info from Utility
Fiberlight-Telecom	\$0.00	\$18,000.00	Preliminary info from Utility
Cobb EMC	\$0.00	\$0.00	Preliminary info from Utility
Georgia Power	\$0.00	\$0.00	Preliminary info from Utility
Marietta Power	\$0.00	\$92,000.00	Preliminary info from Utility
Century Link	\$0.00	\$20,000.00	Site Visit / Available Drawings
AGL	\$0.00	\$7,350.00	Site Visit / Available Drawings
SRTA	\$50,000.00	\$0.00	Site Visit / Available Drawings
Zayo	\$0.00	\$20,000.00	
Total	100.00%	\$50,000.00	\$264,975.00
Department Responsibility	100.00%	\$50,000.00	
Local Sponsor Responsibility	0.00%	\$0.00	PFA Dated N/A with N/A

** Indicates Potential Utility Aid Request from Local Gov't

Estimate is based on the best available information at the current stage, unforeseen prior rights information may be provided by the Utility Company at a later date that could cause some non-reimbursable costs to shift to the reimbursable cost column.

Prepared by Stantec

If additional information is needed, please contact Mark Sweeney at 404-769-6823

cc: Stephen Hopper, Stantec

File

Concept Utility Report

Project Number: 178460151

District: 7B

County: Cobb

Prepared by: Alexa Irvin

***NOTE:** CUR developed by Design Team ~OB

P.I. # 0015049

Date: December 17, 2019

Project Description: The proposed project is to create a shared use path along FranklinGateway to SR 280 (Delk Road) and along delk Road crossing over I-75 via pedestrian bridge. The shared use path continues to Rottenwood Creek and along Rottenwood Creek until it rejoins Delk Road. The shared use path travels down Delk Road until it hits Bentley Road and turns down Bentley Road for short length and terminates. ADA accessible ramps, crosswalks at intersections, landscaping, lighting, seating areas and pedestrian enhancements will be included in this project.

The information provided herein has been gathered from Georgia811and/or field visits and serves as an estimate. Nothing contained in this report is to be used as a substitute for 1st Submission or SUE.

Are SUE services recommended? Yes

Level: A B C D

Public Interest Determination (PID):

Automatic Mandatory Consideration No Use Exempt

Is a separate utility funding phase recommended? No

Potential Project (Schedule/Budget) Impacts: N/A

Capital Improvement Projects (Utilities) Anticipated in the Area: No

Project Specific Recommendations for Avoidance/Mitigation: No

Right of Way Coordination: Yes

Environmental Coordination: In Progress

Additional Remarks: Only minor impacts to existing utilities and appurtenances are known at this time including seven type 1 power poles, existing water meters and valves, telecom manholes need to be adjusted to grade and a few fire hydrants may need to be relocated.

Utilities have facilities within the project limits.

Utilities have been identified using Georgia811 and/or field visits.

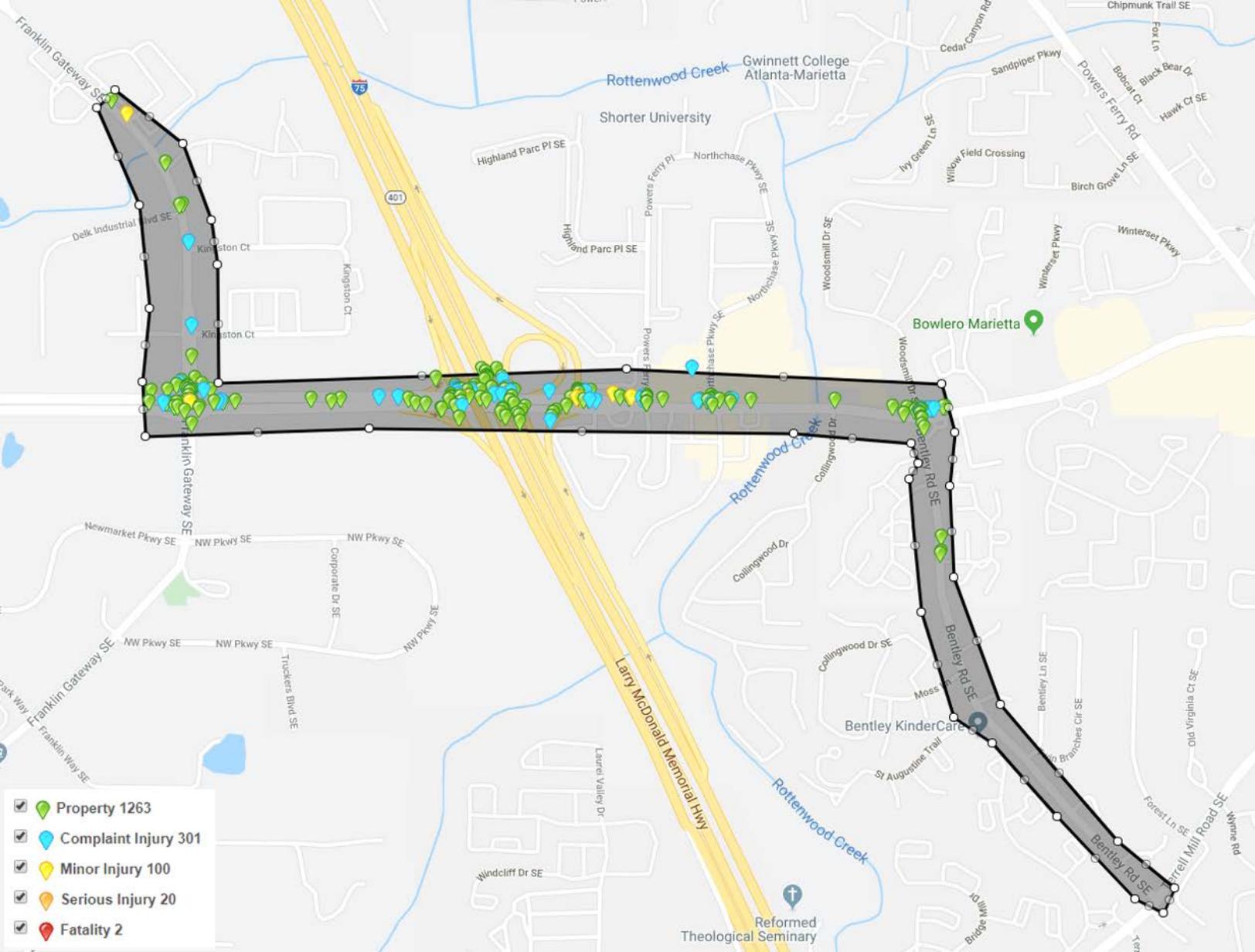
Facility Owner	Facility Owner Contact Email Address	Existing Facilities/ Appurtenances	General Description of Location	Facilities to Avoid <i>approx. limits</i>	Facilities Retention Recommended <i>approx. limits</i>	Comments
Atlanta Gas Light	Andrea Davis 470-464-4055	2", 4" and 6" lines	Along Franklin and Delk	All existing utilities to remain	Requires AGL evaluation	Coordination with utility company will be required.
AT&T	Angelo Hines 305-409-1542	Underground, overhead, fiber, and conduit lines	All on Delk; Conduit, Overhead and Fiber on Franklin	All existing utilities to remain	Yes, within project limits	
Comcast	Oliver Brooks 470-787-4657	Overhead and underground lines	All roads	All facilities to remain	Within project limits	
Cobb County Water	Robbie Johnson 770-419-6255	Underground Lines		All water structures to remain. Some grade adjustment anticipated	Within project limits	Awaiting maps
Cobb County Sewer	Robbie Johnson 770-419-6255	Underground lines		All sewer structures to remain. Some grade adjustment anticipated	Within project limits	Awaiting maps, path utilizing utility easement east of I-75
Cobb County Dept of Transportation Telecom	Robbie Johnson 770-419-6255	Click here to enter text.		All lines and poles to remain	Within project limits	Awaiting maps
Cobb EMC	April Millhollan 678-355-3406	Click here to enter text.		All lines and poles to remain	Within project limits	Awaiting maps

Fiberlight LLC Telecom	Tim Green 727-243-5251 Fiberlight Network Operation Center 800- 672-0181 ext 2	Click here to enter text.		All lines and poles to remain	Within project limits	Awaiting maps
Georgia Power	Georgia Power Cable Locating 404-506-6539	Click here to enter text.		All existing utilities to remain	Within project limits	Evaluation by Georgia Power required
Centurylink	Level 3 Communication 877-366-8344 ext 3	lines	All roads	All existing utilities to remain	Yes, within limits	
Marietta Power	Jeremiah Fields 770-794-5123	Three phase and single phase overhead and underground lines	All roads	Poles and lines anticipated to remain in place	Within project limits	
SRTA		Fiber for Managed Lanes	Adjacent on West side of I- 75		Requires additional SRTA coordination	SUE level investigation, pedestrian bridge could be in conflict

Note: To add additional rows, click the bottom right corner of the box above, then click the blue + that will appear. Please add additional rows prior to entering text.

Attachment 5

Crash Summaries and Diagrams



-  Property 1263
-  Complaint Injury 301
-  Minor Injury 100
-  Serious Injury 20
-  Fatality 2

Franklin Rd SE @ Delk Industrial Blvd

	Angle			Head on			Rear End			Sideswipe - Same			Sideswipe - Opposite			Not a Collision w/ Motor Vehicle			Total	Property Damage Only	Injury	Fatal
	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal				
2014	1						1												2	2	0	0
2015	4	1													1				6	5	1	0
2016	1																		1	1	0	0
2017	2							1		2					1				6	5	1	0
2018	1						2	1		1									5	4	1	0
	10			0			5			3			0			2			20	17	3	

AccidentNo	AccidentNumb	LocalCode	Date	Route	Milelog	IntersectingRoute	MannerOfCollision	FirstHarmfulEvent	Injuries	Fatalities	LatDecimal	LongDecimal	U1VehicleType	U2VehicleType	U2Factors	U2FirstHarmfulEvent	Weather	U1Factors	U1FirstHarmfulEvent
5103009	5103009	0114026660	12/19/2014	FRANKLIN RD		0 DELK INDUSTRIAL BLVD	Rear End	Motor Vehicle In Motion	0	0	33.92616	-84.492171	Passenger Car	Sports Utility Vehic	No Contributing Factors	N/A	Cloudy		
5103013	5103013	0114026560	12/18/2014	FRANKLIN RD		0 KINGSTON CT	Angle	Motor Vehicle In Motion	0	0	33.926143	-84.492234	Passenger Car	Passenger Car	No Contributing Factors	N/A	Clear	Failed to Yield	
5385707	5385707	0115014478	8/6/2015	FRANKLIN RD	0.94	DELK INDUSTRIAL BLVD	Angle	Motor Vehicle In Motion	0	0	33.926165	-84.492161	Pickup Truck	Passenger Car	Other	N/A	Cloudy	Other	
5434293	5434293	0115010964	6/17/2015	FRANKLIN RD		0 DELK INDUSTRIAL BLVD	Angle	Motor Vehicle In Motion	0	0	33.926143	-84.49218	Passenger Car	Passenger Car	No Contributing Factors	N/A	Clear	Failed to Yield	
5437415	5437415	0115014889	8/12/2015	FRANKLIN RD		0 DELK INDUSTRIAL BLVD	Angle	Motor Vehicle In Motion	1	0	33.926151	-84.492201	Passenger Car	Passenger Car	No Contributing Factors	N/A	Clear	Failed to Yield	
5438837	5438837	0115016807	9/9/2015	FRANKLIN RD		0 DELK INDUSTRIAL BLVD	Not A Collision with Motor \	Utility Pole	0	0	33.926174	-84.492242	Single Unit Truck	N/A		N/A	Cloudy	Other	
5439136	5439136	0115017096	9/14/2015	FRANKLIN RD		0 DELK INDUSTRIAL BLVD	Angle	Motor Vehicle In Motion	0	0	33.926146	-84.492209	Passenger Car	Passenger Car	No Contributing Factors	N/A	Clear	Failed to Yield	
5543476	5543476	0115022974	12/8/2015	FRANKLIN RD		0 DELK INDUSTRIAL BLVD	Angle	Motor Vehicle In Motion	0	0	33.926153	-84.4922	Van	Passenger Car	No Contributing Factors	N/A	Clear	Failed to Yield	
6010073	6010073	0116021562	11/22/2016	FRANKLIN GTWY	0.44	DELK INDUSTRIAL BLVD	Angle	Motor Vehicle In Motion	0	0	33.926157	-84.492199	Sports Utility Vehicle (SUV)	Passenger Car	No Contributing Factors	N/A	Cloudy	Changed Lanes Improperly	
6086328	6086328	0117000910	1/13/2017	FRANKLIN GTWY	0.44	DELK INDUSTRIAL BLVD	Angle	Motor Vehicle In Motion	0	0	33.925903	-84.49214	Passenger Car	Sports Utility Vehic	No Contributing Factors	N/A	Clear	Improper Turn	
6242996	6242996	0117009745	5/21/2017	FRANKLIN GTWY		0 DELK INDUSTRIAL BLVD	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0	33.926258	-84.492234	Passenger Car	Passenger Car	No Contributing Factors	N/A	Clear		
6276969	6276969	C000493803	6/13/2017	S MARIETTA PARKWAY	175		Rear End	Motor Vehicle In Motion	1	0	33.92617	-84.49221	N/A	Passenger Car	No Contributing Factors	N/A	Cloudy		
6343117	6343117	0117014755	8/5/2017	FRANKLIN GTWY		0 DELK INDUSTRIAL BLVD	Not A Collision with Motor \	Motor Vehicle In Motion	0	0	33.926291	-84.492245	Pickup Truck	N/A		N/A	Clear	Under the Influence (U.I.),Other	
6444644	6444644	0117020252	10/23/2017	FRANKLIN GTWY		0 DELK INDUSTRIAL BLVD	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0	33.926256	-84.492236	Pickup Truck	Van	No Contributing Factors	N/A	Clear	Changed Lanes Improperly	
6474730	6474730	0117021865	11/14/2017	FRANKLIN GTWY		0 DELK INDUSTRIAL BLVD	Angle	Motor Vehicle In Motion	0	0	33.926101	-84.492191	Passenger Car	Tractor/Trailer	No Contributing Factors	N/A	Clear	Changed Lanes Improperly	
6749984	6749984	0118010333	6/17/2018	FRANKLIN GTWY		0 DELK INDUSTRIAL BLVD	Angle	Motor Vehicle In Motion	0	0	33.926158	-84.492197	Sports Utility Vehicle (SUV)	Passenger Car	No Contributing Factors	Motor Vehicle In Motion	Clear		Motor Vehicle In Motion
6792808	6792808	0118011864	7/12/2018	FRANKLIN GTWY		0 DELK INDUSTRIAL BLVD	Rear End	Motor Vehicle In Motion	0	0	33.926162	-84.492194	Sports Utility Vehicle (SUV)	Sports Utility Vehic	No Contributing Factors	Motor Vehicle In Motion	Clear	Following too Close	Motor Vehicle In Motion
6867522	6867522	0118015448	9/7/2018	FRANKLIN GTWY		0 DELK IND BLVD	Sideswipe-Same Direction		0	0	33.926159	-84.492195	Other	Sports Utility Vehic	No Contributing Factors	Motor Vehicle In Motion	Clear	Misjudged Clearance	Motor Vehicle In Motion
6993476	6993476	0118020522	12/6/2018	FRANKLIN GTWY		0 DELK INDUSTRIAL BLVD	Rear End		1	0	33.926161	-84.492202	Van	Sports Utility Vehic	No Contributing Factors	Motor Vehicle In Motion	Clear	Following too Close	Motor Vehicle In Motion
6993478	6993478	0118020590	12/6/2018	FRANKLIN GTWY		0 DELK INDUSTRIAL BLVD	Rear End		0	0	33.926161	-84.492206	Passenger Car	Passenger Car	No Contributing Factors	Motor Vehicle In Motion	Clear	Following too Close	Motor Vehicle In Motion

Franklin Rd SE @ Kingston Ct. N

	Angle			Head on			Rear End			Sideswipe - Same			Sideswipe - Opposite			Not a Collision w/ Motor Vehicle			Total	Property Damage Only	Injury	Fatal
	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal				
2014	2						1	1		1			1						6	5	1	0
2015	2									1									3	3	0	0
2016		1																	1	0	1	0
2017	1									2									3	3	0	0
2018	2	1					1	3		3			1						11	7	4	0
	7	2	0	0	0	0	2	4	0	7	0	0	2	0	0	0	0	0	24	18	6	0
		9			0			6			7			2			0					

AccidentNo	AccidentNumber	LocalCode	Date	Route	Milelog	IntersectingRoute	MannerOfCollision	FirstHarmfulEvent	Injuries	Fatalities	LatDecimal	LongDecimal	U1VehicleType	U2VehicleType	U1Factors	U2Factors	U2FirstHarmfulEvent	Weather	U1FirstHarmfulEvent
4793985	4793985	114009138	4/12/2014	FRANKLIN RD	0.79	KINGSTON CT	Rear End	Motor Vehicle In Motion	1	0	33.925491	-84.492061	Passenger Car	Passenger Car	Following too Close	No Contributing Factors	N/A	Clear	
4854295	4854295	114011684	5/17/2014	FRANKLIN RD	0	KINGSTON CT	Angle	Motor Vehicle In Motion	0	0	33.925493	-84.49205	Passenger Car	Passenger Car	Improper Turn	No Contributing Factors	N/A	Clear	
4987585	4987585	114020796	9/19/2014	FRANKLIN RD	0	KINGSTON CT	Angle	Motor Vehicle In Motion	0	0	33.925493	-84.49205	Vehicle With Trailer	Van		No Contributing Factors	N/A	Clear	
4997720	4997720	114021148	9/25/2014	FRANKLIN RD	0	KINGSTON CT	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0	33.925493	-84.49205	Sports Utility Vehicle (SUV)	Van	Changed Lanes Improperly	No Contributing Factors	N/A	Rain	
5040150	5040150	114023399	10/27/2014	FRANKLIN RD	0	KINGSTON CT	Sideswipe-Opposite Direction	Motor Vehicle In Motion	0	0	33.925493	-84.49205	Passenger Car	Passenger Car	Under the Influence (U.I.),Wrong Side of Road	No Contributing Factors	N/A	Clear	
5066018	5066018	114025110	11/22/2014	FRANKLIN RD	0	KINGSTON CT	Rear End	Motor Vehicle In Motion	0	0	33.925493	-84.49205	Passenger Car	Passenger Car	Following too Close	No Contributing Factors	N/A	Clear	
5205775	5205775	115003788	3/3/2015	FRANKLIN RD	0	KINGSTON CT	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0	33.925493	-84.49205	Passenger Car	Passenger Car	Changed Lanes Improperly	No Contributing Factors	N/A	Rain	
5435814	5435814	115012529	7/9/2015	FRANKLIN RD	0.89	KINGSTON CT	Angle	Motor Vehicle In Motion	0	0	33.925489	-84.492063	Passenger Car	Passenger Car	Other	Other	N/A	Clear	
5521522	5521522	115022057	11/21/2015	FRANKLIN RD	0.15	KINGSTON CT	Angle	Motor Vehicle In Motion	0	0	33.925203	-84.492082	Sports Utility Vehicle (SUV)	Passenger Car		No Contributing Factors	N/A	Clear	
5687261	5687261	116005568	3/24/2016	FRANKLIN GTWY	0.89	KINGSTON CT	Angle	Motor Vehicle In Motion	2	0	33.925491	-84.492068	Passenger Car	Passenger Car	Failed to Yield	No Contributing Factors	N/A	Cloudy	
6086328	6086328	117000910	1/13/2017	FRANKLIN GTWY	0.44	DELK INDUSTRIAL BLVD	Angle	Motor Vehicle In Motion	0	0	33.925903	-84.49214	Passenger Car	Sports Utility Vehicle (SUV)	Improper Turn	No Contributing Factors	N/A	Clear	
6186314	6186314	117006895	4/8/2017	FRANKLIN GTWY	0	KINGSTON CT	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0	33.925493	-84.49205	Van	Passenger Car		No Contributing Factors	N/A	Clear	
6442954	6442954	117019975	10/19/2017	FRANKLIN GTWY	0	KINGSTON CT	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0	33.925493	-84.49205	Passenger Car	Passenger Car		No Contributing Factors	N/A	Clear	
6599953	6599953	118002428	2/10/2018	FRANKLIN GTWY	0	KINGSTON CT	Rear End	Motor Vehicle In Motion	1	0	33.925497	-84.492047	Passenger Car	Passenger Car	Following too Close	No Contributing Factors	N/A	Rain	
6635355	6635355	118004350	3/15/2018	FRANKLIN GTWY	0	KINGSTON CT	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0	33.925497	-84.492047	Sports Utility Vehicle (SUV)	Pickup Truck		No Contributing Factors	N/A	Clear	
6638229	6638229	118004446	3/16/2018	FRANKLIN GTWY	0		Sideswipe-Same Direction	Motor Vehicle In Motion	0	0	33.92549	-84.492116	Passenger Car	Pickup Truck	Changed Lanes Improperly	No Contributing Factors	N/A	Cloudy	
6714028	6714028	118008307	5/16/2018	FRANKLIN GTWY	0	KINGSTON CT	Angle	Motor Vehicle In Motion	0	0	33.925497	-84.492047	Sports Utility Vehicle (SUV)	Passenger Car	Changed Lanes Improperly	No Contributing Factors	Motor Vehicle In Motion	Rain	Motor Vehicle In Motion
6729242	6729242	118009080	5/30/2018	FRANKLIN GTWY	0	KINGSTON CT	Sideswipe-Opposite Direction	Motor Vehicle In Motion	0	0	33.92549	-84.492116	Passenger Car	Passenger Car		No Contributing Factors	Motor Vehicle In Motion	Rain	Motor Vehicle In Motion
6729573	6729573	118009079	5/30/2018	FRANKLIN GTWY	0	KINGSTON CT	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0	33.92549	-84.492116	Sports Utility Vehicle (SUV)	Passenger Car	Changed Lanes Improperly	No Contributing Factors	Motor Vehicle In Motion	Rain	Motor Vehicle In Motion
6747021	6747021	118009982	6/13/2018	FRANKLIN GTWY	0	KINGSTON CT	Angle	Motor Vehicle In Motion	0	0	33.92549	-84.492116	Passenger Car	Tractor/Trailer	Improper Passing	No Contributing Factors	Motor Vehicle In Motion	Cloudy	Motor Vehicle In Motion
6776251	6776251	118011142	6/30/2018	FRANKLIN GATEWAT	0	KINGSTON CT	Rear End		0	0	33.925208	-84.492071	Passenger Car	Passenger Car	Following too Close	No Contributing Factors	Motor Vehicle In Motion	Clear	Motor Vehicle In Motion
6876620	6876620	118015942	9/14/2018	FRANKLIN GTWY	0	FRANKLIN WAY	Angle		1	0	33.92549	-84.492116	Passenger Car	Motorcycle	Changed Lanes Improperly	No Contributing Factors	Motor Vehicle In Motion	Cloudy	Motor Vehicle In Motion
6885853	6885853	118016499	9/22/2018	FRANKLIN GTWY	0	KINGSTON CT	Rear End		1	0	33.925497	-84.492047	Passenger Car	Sports Utility Vehicle (SUV)		No Contributing Factors	Motor Vehicle In Motion	Clear	Motor Vehicle In Motion
6997654	6997654	118020693	12/8/2018	KINGSTON CT	0	FRANKLIN GTWY	Rear End		1	0	33.925498	-84.492075	Passenger Car	Passenger Car		No Contributing Factors	Motor Vehicle In Motion	Rain	Motor Vehicle In Motion

Franklin Rd SE @ Kingston Ct. N

	Angle			Head on			Rear End			Sideswipe - Same			Sideswipe - Opposite			Not a Collision w/ Motor Vehicle			Total	Property Damage Only	Injury	Fatal	
	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal					
2014								1											1	0	1	0	
2015	1						1			2									4	4	0	0	
2016	1						1												2	2	0	0	
2017	1						1			1									3	3	0	0	
2018	1									1					1				3	3	0	0	
	4	0	0	0	0	0	3	1	0	4	0	0	0	0	0	0	0	1	13	12	1	0	
		4			0			4			4			0			1						

AccidentNo	AccidentNumber	LocalCode	Date	Time	County	Route	Milelog	IntersectingRoute	MannerOfCollision	FirstHarmfulEvent	Injuries	Fatalities	U2Factors
5012099	5012099	0114021867	10/6/2014	19:20:00	COBB	FRANKLIN RD		0 KINGSTON CT	Rear End	Motor Vehicle In Motion	1	0	No Contributing Factors
5339799	5339799	0115011776	6/28/2015	10:55:00	COBB	FRANKLIN RD		0 KINGSTON CT	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0	Changed Lanes Improperly
5433371	5433371	0115009118	5/22/2015	19:05:00	COBB	FRANKLIN RD	0.79	DELK RD	Rear End	Motor Vehicle In Motion	0	0	No Contributing Factors
5434294	5434294	0115011020	6/18/2015	11:37:00	COBB	FRANKLIN RD	0.79	KINGSTON CT	Angle	Motor Vehicle In Motion	0	0	No Contributing Factors
5488241	5488241	0115020501	10/29/2015	7:30:00	COBB	FRANKLIN RD	0.89	KINGSTON CT	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0	Changed Lanes Improperly
5862310	5862310	C000426034	8/3/2016	8:59:00	COBB	AT S MARIETTA I 75 PKWY S	0.84		Rear End	Motor Vehicle In Motion	0	0	No Contributing Factors
5997502	5997502	0116020953	11/13/2016	9:25:00	COBB	FRANKLIN GTWY	0.13	KINGSTON CT	Angle	Motor Vehicle In Motion	0	0	No Contributing Factors
6289756	6289756	0117012055	6/27/2017	10:26:00	COBB	FRANKLIN GTWY		0 DELK RD	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0	No Contributing Factors
6405271	6405271	0117018333	9/25/2017	7:17:00	COBB	FRANKLIN GTWY		0 DELK RD	Angle	Motor Vehicle In Motion	0	0	No Contributing Factors
6432047	6432047	0117019803	10/16/2017	9:05:00	COBB	FRANKLIN GTWY		0 DELK RD	Rear End	Motor Vehicle In Motion	0	0	No Contributing Factors
6617442	6617442	C000542552	2/16/2018	16:35:00	COBB	N OF WINDY HILL I 75 RD N		0	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0	No Contributing Factors
6661550	6661550	C000554631	4/6/2018	5:10:00	COBB	S I285 I 75		0	Not A Collision with Motor Vehicle	Other Object (Not Fixed)	0	0	
7024047	7024047	0118020753	12/9/2018	18:19:00	COBB	FRANKLIN GTWY		0 KINGSTON CT	Angle		0	0	No Contributing Factors

Delk Rd @ Franklin Gateway SE

	Angle			Head on			Rear End			Sideswipe - Same			Sideswipe - Opposite			Not a Collision w/ Motor Vehicle			Total	Property Damage Only	Injury	Fatal	
	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal					
2014	9	6					39	13		8	3					1			79	57	22	0	
2015	9	5		2			44	10		13	1					1			85	69	16	0	
2016	15	8					43	14		8	1					1	1		91	67	24	0	
2017	17	3			1		39	14		10	3								87	66	21	0	
2018	10	2			1		30	20		13						2			78	55	23	0	
	60	24	0	2	2	0	195	71	0	52	8	0	0	0	0	5	1	0	420	314	106		
		84			4			266			60			0			6						

AccidentNo	AccidentNumber	LocalCode	Date	Time	County	Route	Milelog	RampSection	IntersectingRoute	MannerOfCollision	Injuries	Fatalities
6540339	6540339	011800109	1/3/2018	16:24:00	COBB	DELK RD		0	0 FRANKLIN GAYEWAY	Rear End	0	0
6549357	6549357	0118000523	1/11/2018	12:40:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Sideswipe-Same Direction	0	0
6553273	6553273	0118000581	1/12/2018	10:20:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6566095	6566095	0118001227	1/24/2018	8:18:00	COBB	DELK RD		0	0	Rear End	1	0
6566097	6566097	0118001232	1/24/2018	8:42:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Sideswipe-Same Direction	0	0
6574689	6574689	0118001796	1/31/2018	12:45:00	COBB	FRANKLIN GTWY		0	0	Head On	2	0
6583499	6583499	0118002128	2/5/2018	17:53:00	COBB	FRANKLIN GTWY		0	0	Sideswipe-Same Direction	0	0
6585095	6585095	0118002251	2/8/2018	8:43:00	COBB	DELK RD		0	0	Rear End	1	0
6596701	6596701	C000502816	2/16/2018	16:17:00	COBB	N OF DELK I 75 RD S		0	0	Rear End	0	0
6599980	6599980	0118002748	2/16/2018	19:15:00	COBB	FRANKLIN GTWY		0	0	Rear End	0	0
6609752	6609752	0118003132	2/23/2018	11:39:00	COBB	DELK RD		0	0	Rear End	0	0
6615759	6615759	0118003503	3/1/2018	7:55:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	1	0
6617442	6617442	C000542552	2/16/2018	16:35:00	COBB	N OF WINDY HILL I 75 RD N		0	0	Sideswipe-Same Direction	0	0
6620857	6620857	0118003697	3/5/2018	8:15:00	COBB	DELK RD		0	0	Rear End	1	0
6623418	6623418	0118003790	3/6/2018	17:45:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6628016	6628016	0118003806	3/6/2018	23:43:00	COBB	DELK RD		0	0	Angle	1	0
6629597	6629597	0118003981	3/8/2018	19:30:00	COBB	FRANKLIN GTWY		0	0	Rear End	0	0
6632843	6632843	0118003980	3/8/2018	19:00:00	COBB	FRANKLIN GTWY		0	0	Rear End	1	0
6643160	6643160	0118004537	3/17/2018	21:10:00	COBB	DELK RD		0	0	Angle	0	0
6656434	6656434	0118005530	4/2/2018	9:14:00	COBB	SR 280		0	0	Angle	0	0
6660494	6660494	0118005632	4/3/2018	17:20:00	COBB	FRANKLIN GTWY		0	0	Rear End	0	0
6661527	6661527	C000550549	4/4/2018	10:15:00	COBB	S EXIT DELK I 75 RD		0	1	Sideswipe-Same Direction	0	0
6669599	6669599	0118006049	4/11/2018	1:25:00	COBB	DELK RD		0	0	Sideswipe-Same Direction	0	0
6669799	6669799	0118006062	4/11/2018	8:40:00	COBB	DELK RD		0	0	Rear End	2	0
6685925	6685925	0118007027	4/25/2018	13:40:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Sideswipe-Same Direction	0	0
6689804	6689804	0118006674	4/20/2018	4:47:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Rear End	0	0
6690856	6690856	0118007155	4/27/2018	19:37:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	1	0
6698170	6698170	0118007574	5/4/2018	15:00:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Sideswipe-Same Direction	0	0
6711396	6711396	0118008163	5/13/2018	6:00:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Sideswipe-Same Direction	0	0
6712020	6712020	0118008272	5/15/2018	15:56:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6715739	6715739	0118008408	5/17/2018	18:36:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Angle	0	0
6729017	6729017	0118008876	5/25/2018	17:15:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Rear End	1	0
6729019	6729019	0118008970	5/27/2018	22:49:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	2	0
6732706	6732706	0118009135	5/30/2018	19:00:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Rear End	0	0
6737877	6737877	0118009501	6/5/2018	8:00:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6745615	6745615	0118009932	6/12/2018	7:00:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Rear End	0	0
6764166	6764166	0118010427	6/19/2018	15:00:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Angle	0	0
6770063	6770063	0118010737	6/24/2018	19:42:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Not A Collision with Motor Vehicle	0	0
6770984	6770984	0118010813	6/25/2018	19:00:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Angle	0	0
6778776	6778776	0118011189	7/1/2018	13:11:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Angle	0	0
6782487	6782487	0118011160	6/30/2018	19:00:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6786459	6786459	0118011439	7/5/2018	19:45:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Rear End	0	0
6786462	6786462	0118011552	7/7/2018	23:41:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	1	0
6789016	6789016	0118011747	7/11/2018	16:50:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Angle	1	0
6789113	6789113	0118011644	7/10/2018	8:40:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Angle	0	0
6789179	6789179	0118011864	7/10/2018	19:04:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	1	0
6812930	6812930	0118012726	7/25/2018	13:58:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6812991	6812991	0118013060	7/28/2018	19:53:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6822854	6822854	0118013105	7/29/2018	20:00:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6824238	6824238	0118013477	8/6/2018	8:23:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Rear End	0	0
6825273	6825273	0118013472	8/6/2018	8:04:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	1	0
6827161	6827161	0118013758	8/10/2018	6:50:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6839675	6839675	0118014061	8/15/2018	16:41:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Sideswipe-Same Direction	0	0
6841348	6841348	0118014379	8/21/2018	8:03:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6842258	6842258	18082163	8/20/2018	23:19:00	COBB	FRANKLIN GTWY		0	0 SR 280	Rear End	1	0
6845540	6845540	0118014371	8/20/2018	23:15:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Angle	0	0
6845545	6845545	0118014512	8/23/2018	9:35:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6871851	6871851	0118015714	9/11/2018	18:30:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Rear End	1	0
6872759	6872759	0118015596	9/10/2018	2:15:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Angle	0	0
6876041	6876041	0118016061	9/16/2018	16:15:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	1	0
6897192	6897192	0118016948	9/30/2018	18:00:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6917637	6917637	0118017801	10/15/2018	7:20:00	COBB	DELK RD		0	0 FRANKLIN RD	Rear End	0	0
6929694	6929694	0118018324	10/24/2018	20:45:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Not A Collision with Motor Vehicle	0	0
6930585	6930585	0118018280	10/24/2018	7:21:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6933190	6933190	0118018440	10/26/2018	16:41:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	2	0
6937526	6937526	0118018602	10/30/2018	11:40:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Sideswipe-Same Direction	0	0
6944325	6944325	0118018775	11/2/2018	14:32:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Sideswipe-Same Direction	0	0
6951876	6951876	0118018952	11/6/2018	7:35:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Sideswipe-Same Direction	0	0
6952230	6952230	0118018965	11/6/2018	9:34:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	1	0
6965693	6965693	0118019417	11/15/2018	21:25:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Rear End	0	0
6980512	6980512	0118020028	11/27/2018	21:00:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6986295	6986295	0118020104	11/29/2018	8:08:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	1	0
6987989	6987989	0118020323	12/2/2018	19:26:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
6998443	6998443	0118020765	12/10/2018	6:33:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Angle	0	0
7024067	7024067	0118021235	12/18/2018	7:23:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	1	0
7024085	7024085	0118021304	12/19/2018	6:47:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	0	0
7024230	7024230	0118021886	12/31/2018	19:56:00	COBB	FRANKLIN GTWY		0	0 DELK RD	Rear End	0	0
7034482	7034482	0118021234	12/18/2018	7:23:00	COBB	DELK RD		0	0 FRANKLIN GTWY	Rear End	1	0

Delk Rd @ Collingwood Dr

	Angle			Head on			Rear End			Sideswipe - Same			Sideswipe - Opposite			Not a Collision w/ Motor Vehicle			Total	Property Damage Only	Injury	Fatal	
	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal					
2014	2	1					1												4	3	1	0	
2015	1	1																	4	1	3	0	
2016	3	1			1		7			1									14	11	3	0	
2017	5	3						1		1									10	6	4	0	
2018	3	1					1	1											6	4	2	0	
	14	7	0	0	1	0	9	2	0	2	0	0	0	0	0	0	3	0	38	25	13	0	
		21			1			11			2			0			3						

LocalCode	Date	Time	County	Route	Milelog	RampSection	IntersectingRoute	MannerOfCollision	FirstHarmfulEvent	Injuries	Fatalities
114008360	3/31/2014	12:45:00	COBB	DELK RD	0.31		0 COLLINGWOOD DR	Angle	Motor Vehicle In Motion	0	0
14035784	4/29/2014	14:40:00	COBB	DELK RD	0		0 BENTLEY RD	Angle	Motor Vehicle In Motion	1	0
14048339	6/7/2014	8:04:00	COBB	DELK RD	0.31		0 COLLINGWOOD DR	Angle	Motor Vehicle In Motion	0	0
14058783	7/11/2014	18:12:00	COBB	DELK RD	0		0 2560 DELK RD	Rear End	Motor Vehicle In Motion	0	0
15081801	8/28/2015	20:56:00	COBB	DELK RD	0		0	Not A Collision with Motor Vehicle	Pedestrian	1	0
115009538	5/28/2015	21:25:00	COBB	DELK RD	0.32		0 BENTLEY RD	Not A Collision with Motor Vehicle	Pedestrian	1	0
115018755	10/6/2015	7:45:00	COBB	DELK RD	0		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
15109722	11/18/2015	17:04:00	COBB	DELK RD	0		0	Angle	Motor Vehicle In Motion	2	0
16035562	4/12/2016	19:02:00	COBB	DELK RD	0		0	Rear End	Motor Vehicle In Motion	0	0
116007222	4/18/2016	16:20:00	COBB	DELK RD	0.32		0 COLLINGWOOD DR	Angle	Motor Vehicle In Motion	2	0
116010050	5/26/2016	18:30:00	COBB	DELK RD	0.33		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
116012237	6/28/2016	17:48:00	COBB	DELK RD	0.24		0 BENTLEY RD	Not A Collision with Motor Vehicle	Pedestrian	1	0
116012466	7/1/2016	17:30:00	COBB	DELK RD	0.24		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
116012465	7/1/2016	17:30:00	COBB	DELK RD	0.24		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
116017294	9/14/2016	15:00:00	COBB	DELK RD	0.3		0 NORTHCHASE PKWY	Rear End	Motor Vehicle In Motion	0	0
116017563	9/19/2016	8:20:00	COBB	DELK RD	0.3		0 NORTHCHASE PKWY	Rear End	Motor Vehicle In Motion	0	0
116017440	9/16/2016	17:33:00	COBB	DELK RD	0		0 ENT TO 2582 DELK RD	Head On	Motor Vehicle In Motion	2	0
116019361	10/17/2016	18:45:00	COBB	DELK RD	0		0 COLLINGWOOD DR	Angle	Motor Vehicle In Motion	0	0
116019635	10/21/2016	20:20:00	COBB	DELK RD	0.24		0 BENTLEY RD	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0
116020893	11/11/2016	19:18:00	COBB	DELK RD	0.24		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
116021942	11/29/2016	17:08:00	COBB	DELK RD	1.34		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
116022399	12/6/2016	17:42:00	COBB	DELK RD	0.3		0 NORTHCHASE PKWY	Rear End	Motor Vehicle In Motion	0	0
17007663	1/26/2017	8:40:00	COBB	DELK RD	0.29		0 COLLINGWOOD DR	Angle	Motor Vehicle In Motion	2	0
17008043	1/27/2017	12:21:00	COBB	DELK RD	2.21		0 POWERS FERRY RD	Angle	Motor Vehicle In Motion	0	0
117002457	2/6/2017	18:00:00	COBB	DELK RD	0.32		0 COLLINGWOOD DR	Angle	Motor Vehicle In Motion	0	0
117003704	2/23/2017	20:06:00	COBB	DELK RD	0		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
17023980	3/16/2017	6:54:00	COBB	DELK RD	0.16		0 NORTHCHASE PKWY	Angle	Motor Vehicle In Motion	0	0
117012613	7/5/2017	20:16:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	2	0
117016231	8/25/2017	17:28:00	COBB	DELK RD	0		0 POWERS FERRY RD	Angle	Motor Vehicle In Motion	0	0
17085145	9/3/2017	23:10:00	COBB	DELK RD	0		0 COLLINGWOOD DR	Angle	Motor Vehicle In Motion	1	0
117023257	12/7/2017	7:20:00	COBB	DELK RD	0		0 COLLINGWOOD DR	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0
117024482	12/27/2017	21:14:00	COBB	DELK RD	0		0 COLLINGWOOD DR	Angle	Motor Vehicle In Motion	1	0
118006320	4/14/2018	18:40:00	COBB	DELK RD	0		0 COLLINGWOOD DR	Rear End	Motor Vehicle In Motion	0	0
118009232	5/31/2018	16:20:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	1	0
118011925	7/13/2018	22:00:00	COBB	DELK RD	0		0 BENTLEY RD	Angle		0	0
118013269	8/1/2018	21:00:00	COBB	DELK RD	0		0 BENTLEY RD	Angle		2	0
118014410	8/21/2018	16:10:00	COBB	DELK RD	0		0 BENTLEY	Angle		0	0
118019903	11/24/2018	18:30:00	COBB	DELK RD	0.16		0 NORTHCHASE PKWY	Angle		0	0

Delk Rd @ Bentley Rd SE

	Angle			Head on			Rear End			Sideswipe - Same			Sideswipe - Opposite			Not a Collision w/ Motor Vehicle			Total	Property Damage Only	Injury	Fatal	
	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal	PDO	Injury	Fatal					
2014	8	1			1		9	6		1	1					2			29	20	9	0	
2015	11	4			1		5	2		3						2	1		29	21	8	0	
2016	9	1					6	4		5							1		26	20	6	0	
2017	9	5					9	5			2					1	2		33	19	14	0	
2018	4	5			1		11	3		1									25	16	9	0	
	41	16	0	0	3	0	40	20	0	10	3	0	0	0	0	5	4	0	142	96	46	0	
		57			3			60			13			0			9						

AccidentNo	AccidentNumber	LocalCode	Date	Time	County	Route	Milelog	RampSection	IntersectingRoute	MannerOfCollision	FirstHarmfulEvent	Injuries	Fatalities
4707893	4707893	0114003050	1/15/2014	7:30:00	COBB	DELK RD	0.24		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
4720983	4720983	0114004012	1/28/2014	8:30:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
4748158	4748158	0114006190	3/1/2014	9:30:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
4749226	4749226	14017195	2/28/2014	23:52:00	COBB	DELK RD	0.24		0 BENTLEY RD	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0
4776735	4776735	0114008208	3/28/2014	1:27:00	COBB	DELK RD	0.24		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
4785186	4785186	0114008745	4/6/2014	19:10:00	COBB	DELK RD	0.24		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	2	0
4845574	4845574	0114010419	5/1/2014	19:35:00	COBB	DELK RD	0.24		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
4871055	4871055	0114012562	5/31/2014	2:44:00	COBB	BENTLEY RD	0		0 DELK RD	Not A Collision with Motor Vehicle	Utility Pole	0	0
4873689	4873689	0114013340	6/10/2014	18:40:00	COBB	DELK RD	0.24		0 BENTLEY RD	Angle	Motor Vehicle In Motion	1	0
4895842	4895842	0114014831	6/30/2014	14:15:00	COBB	DELK RD	0		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
4904482	4904482	0114015422	7/9/2014	13:00:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	1	0
4913112	4913112	0114015597	7/11/2014	14:50:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	1	0
4913189	4913189	0114016039	7/17/2014	19:50:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
4915541	4915541	0114016228	7/21/2014	8:30:00	COBB	BENTLEY RD	0		0 DELK RD	Angle	Motor Vehicle In Motion	0	0
4923989	4923989	0114016690	7/29/2014	18:00:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
4964922	4964922	0114019489	9/2/2014	16:30:00	COBB	BENTLEY RD	0		0 DELK RD	Rear End	Motor Vehicle In Motion	0	0
4966381	4966381	0114019634	9/5/2014	16:00:00	COBB	BENTLEY RD	0		0 DELK RD	Angle	Motor Vehicle In Motion	0	0
4972871	4972871	0114019884	9/7/2014	14:00:00	COBB	DELK RD	0.24		0 BENTLEY RD	Not A Collision with Motor Vehicle	Motor Vehicle In Motion	0	0
4978119	4978119	0114020063	9/9/2014	19:55:00	COBB	DELK RD	0		0 BENTLEY RD	Head On	Motor Vehicle In Motion	4	0
4987568	4987568	0114020684	9/19/2014	15:18:00	COBB	DELK RD	0.24		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	2	0
5021311	5021311	0114022398	10/16/2014	21:00:00	COBB	BENTLEY RD	0.24		0 DELK RD	Angle	Motor Vehicle In Motion	0	0
5021779	5021779	0114022681	10/18/2014	8:15:00	COBB	BENTLEY RD	0.24		0 DELK RD	Rear End	Motor Vehicle In Motion	1	0
5037885	5037885	0114023765	11/1/2014	20:17:00	COBB	DELK RD	0.42		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
5052286	5052286	0114024485	11/13/2014	17:25:00	COBB	DELK RD	0		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
5066519	5066519	0114024756	11/18/2014	20:35:00	COBB	DELK RD	0.24		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
5103007	5103007	0114026643	12/19/2014	14:40:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	1	0
5103038	5103038	0114026393	12/16/2014	17:24:00	COBB	DELK RD	0		0 BENTLEY RD	Sideswipe-Same Direction	Motor Vehicle In Motion	1	0
5105854	5105854	0114026821	12/23/2014	6:49:00	COBB	BENTLEY RD	0		0 DELK RD	Rear End	Motor Vehicle In Motion	0	0
5107435	5107435	0114026897	12/24/2014	14:40:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
5164304	5164304	0115001290	1/23/2015	0:56:00	COBB	BENTLEY RD	0		0 DELK RD	Not A Collision with Motor Vehicle	Utility Pole	1	0
5173841	5173841	0115002549	2/11/2015	7:20:00	COBB	DELK RD	0		0 BENTLEY RD	Not A Collision with Motor Vehicle	Motor Vehicle In Motion	0	0
5177916	5177916	0115002619	2/12/2015	8:55:00	COBB	DELK RD	0		0 I 75	Rear End	Motor Vehicle In Motion	0	0
5178703	5178703	0115002616	2/12/2015	7:30:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	1	0
5184468	5184468	0115002807	2/14/2015	22:00:00	COBB	DELK RD	0		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
5193026	5193026	0115003219	2/23/2015	7:20:00	COBB	BENTLEY RD	0		0 DELK RD	Angle	Motor Vehicle In Motion	0	0
5210738	5210738	0115003816	3/4/2015	9:15:00	COBB	DELK RD	0		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
5217325	5217325	0115004449	3/13/2015	15:17:00	COBB	DELK RD	0		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
5217334	5217334	0115004458	3/13/2015	18:38:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
5226717	5226717	0115005023	3/22/2015	12:31:00	COBB	DELK RD	0		0 POWERS FERRY PL	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0
5228683	5228683	0115005004	3/21/2015	22:49:00	COBB	DELK RD	0		0 POWERS FERRY PL	Angle	Motor Vehicle In Motion	0	0
5249798	5249798	15032128	4/10/2015	19:05:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
5254502	5254502	1501899	3/25/2015	10:04:00	COBB	BENTLEY RD	0		0 DELK RD	Angle	Motor Vehicle In Motion	0	0
5282119	5282119	0115008013	5/8/2015	7:30:00	COBB	DELK RD	0		0 BENTLEY RD	Head On	Motor Vehicle In Motion	3	0
5288359	5288359	0115008299	5/13/2015	9:00:00	COBB	BENTLEY DR	0		0 DELK RD	Angle	Motor Vehicle In Motion	1	0
5385710	5385710	0115014554	8/7/2015	19:19:00	COBB	DELK RD	0.24		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
5400664	5400664	15079707	8/23/2015	15:39:00	COBB	DELK RD	0		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
5434040	5434040	0115010231	6/5/2015	16:30:00	COBB	DELK RD	0.23		0 BENTLEY RD	Not A Collision with Motor Vehicle	Curb	0	0
5435376	5435376	0115011902	6/30/2015	15:27:00	COBB	DELK RD	0.23		0 BENTLEY RD	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0
5437233	5437233	0115014547	8/7/2015	17:20:00	COBB	DELK RD	0.44		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	1	0
5438606	5438606	0115016497	9/3/2015	20:38:00	COBB	BENTLEY RD	0.24		0 DELK RD	Angle	Motor Vehicle In Motion	0	0
5460922	5460922	0115018777	10/6/2015	12:50:00	COBB	BENTLEY RD	0.24		0 DELK RD	Angle	Motor Vehicle In Motion	0	0
5465903	5465903	0115019152	10/11/2015	15:54:00	COBB	DELK RD	0		0 BENTLEY RD	Angle	Motor Vehicle In Motion	1	0
5467199	5467199	0115019147	10/11/2015	14:02:00	COBB	DELK RD	0.23		0 BENTLEY RD	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0
5487588	5487588	0115020331	10/26/2015	19:24:00	COBB	BENTLEY RD	0.24		0 DELK RD	Rear End	Motor Vehicle In Motion	0	0
5512684	5512684	0115021570	11/12/2015	23:10:00	COBB	DELK RD	0.23		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
5514026	5514026	0115021556	11/12/2015	18:34:00	COBB	DELK RD	0.23		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
5540296	5540296	0115022914	12/7/2015	6:35:00	COBB	DELK RD	0		0 BENTLEY RD	Angle	Motor Vehicle In Motion	2	0
5562284	5562284	0115023830	12/18/2015	23:54:00	COBB	DELK RD	0.23		0 BENTLEY RD	Angle	Motor Vehicle In Motion	1	0
5593806	5593806	0116000540	1/9/2016	17:41:00	COBB	DELK RD	0.23		0 BENTLEY RD	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0
5601139	5601139	0116000946	1/15/2016	11:37:00	COBB	DELK RD	0.23		0 BENTLEY RD	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0
5628911	5628911	0116002421	2/6/2016	18:30:00	COBB	DELK RD	0		0 BENTLEY RD	Angle	Motor Vehicle In Motion	1	0
5635681	5635681	0116002706	2/11/2016	16:43:00	COBB	DELK RD	0.23		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
5638025	5638025	0116002819	2/12/2016	22:40:00	COBB	BENTLEY RD	0.24		0 DELK RD	Rear End	Motor Vehicle In Motion	0	0
5662705	5662705	0000391223	3/4/2016	14:39:00	COBB	S OF DELK I 75 RD N	0.24		0	Angle	Other Object (Not Fixed)	0	0
5667381	5667381	0116004455	3/9/2016	7:56:00	COBB	BENTLEY RD	0.24		0 DELK RD	Angle	Motor Vehicle In Motion	0	0
5692865	5692865	0116005823	3/28/2016	13:00:00	COBB	BENTLEY RD	0.24		0 DELK RD	Angle	Motor Vehicle In Motion	0	0
5694711	5694711	0116005608	3/24/2016	20:36:00	COBB	BENTLEY RD	0.24		0 DELK RD	Angle	Motor Vehicle In Motion	0	0
5717920	5717920	0116006986	4/14/2016	21:41:00	COBB	DELK RD	0.23		0 BENTLEY RD	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0
5720022	5720022	0116007301	4/19/2016	16:00:00	COBB	DELK RD	0.23		0 BENTLEY RD	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0
5743594	5743594	0116008557	5/6/2016	6:38:00	COBB	DELK RD	0.23		0 BENTLEY RD	Not A Collision with Motor Vehicle	Other Non-Collision	1	0
5751789	5751789	0116008440	5/4/2016	17:32:00	COBB	DELK RD	0.23		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
5773611	5773611	0116010086	5/27/2016	12:00:00	COBB	BENTLEY RD	0.24		0 DELK RD	Rear End	Motor Vehicle In Motion	1	0
5790166	5790166	16056100	6/7/2016	21:22:00	COBB	BENTLEY RD	0.24		0 DELK RD	Rear End	Motor Vehicle In Motion	4	0
5793953	5793953	0116011023	6/11/2016	16:48:00	COBB	BENTLEY RD	0.24		0 DELK RD	Rear End	Motor Vehicle In Motion	0	0
5793958	5793958	0116011093	6/12/2016	19:02:00	COBB	DELK RD	0.24		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
5843881	5843881	0116013545	7/20/2016	17:08:00	COBB	BENTLEY RD	0.24		0 DELK RD	Rear End	Motor Vehicle In Motion	0	0
5845699	5845699	0116013276	7/15/2016	21:36:00	COBB	BENTLEY RD	0.24		0 DELK RD	Angle	Motor Vehicle In Motion	0	0
5846355	5846355	0116013578	7/21/2016	8:33:00	COBB	BENTLEY RD	0.24		0 DELK RD	Rear End	Motor Vehicle In Motion	3	0
5897860	5897860	0116016220	8/29/2016	7:44:00	COBB	BENTLEY RD	0.08		0 DELK RD	Rear End	Motor Vehicle In Motion	0	0
5902821	5902821	0116016376	8/31/2016	10:36:00	COBB	TERRELL MILL RD	0		0 BENTLEY RD	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0
5934399	5934399	0000440544	9/23/2016	12:51:00	COBB	DELK RD	0.24		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
5996867	5996867	0116020881	11/11/2016	14:50:00	COBB	BENTLEY RD	0.24		0 DELK RD	Angle	Motor Vehicle In Motion	0	0
6035489	6035489	0116022408	12/6/2016	19:52:00	COBB	DELK RD	1.34		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	0	0
6037535	6037535	0116022337	12/5/2016	20:00:00	COBB	BENTLEY RD	0.24		0 DELK RD	Rear End	Motor Vehicle In Motion	1	0
6092907	6092907	0117001573	1/23/2017	20:00:00	COBB	DELK RD	1.34		0 BENTLEY RD	Angle	Motor Vehicle In Motion	0	0
6131436	6131436	0117003761	2/24/2017	11:05:00	COBB	BENTLEY RD	0.24		0 DELK RD	Angle	Motor Vehicle In Motion	0	0
6152797	6152797	0117005199	3/15/2017	16:46:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	1	0
6186090	6186090	0117006801	4/7/2017	12:00:00	COBB	DELK RD	0		0 BENTLEY RD	Sideswipe-Same Direction	Motor Vehicle In Motion	0	0
6186308	6186308	0117006839	4/7/2017	20:50:00	COBB	DELK RD	0		0 BENTLEY RD	Angle	Motor Vehicle In Motion	2	0
6196675	6196675	0117007277	4/14/2017	16:36:00	COBB	DELK RD	0		0 BENTLEY RD	Rear End	Motor Vehicle In Motion	1	0
6202358	6202358	0117007556	4/19/2017	8:									

Attachment 6

Capacity Analysis Summary

**Rottenwood Creek Trail Phase II-Scoping Only
PI 0015049**

City of Marietta, Cobb County, GA



Prepared for:
City of Marietta, Georgia

Prepared by:
Stantec Consulting Services, Inc.

March 27, 2020

Existing Capacity Analysis

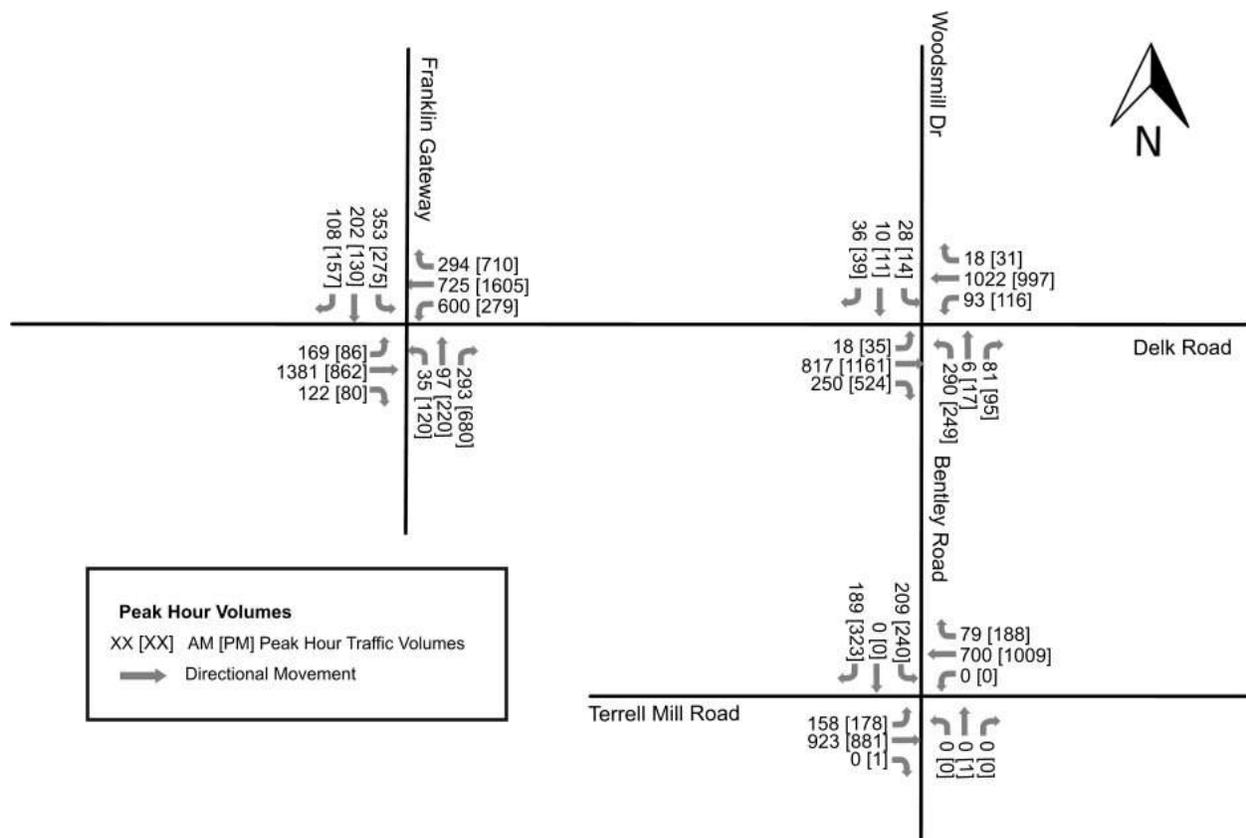
Existing capacity analyses of several intersections were completed using procedures in the Transportation Research Board’s Highway Capacity Manual (HCM). The signalized intersections that were studied are:

- Delk Road & Bentley Road/Woodsmill Drive
- Delk Road & Franklin Gateway
- Terrell Mill Road & Bentley Road

The software program Synchro 10 was used to perform the actual capacity analyses of the key intersections. The existing signal timings and signal clearance times were used in this analysis.

The analysis was based on peak hour turning movement counts that were conducted on Wednesday, May 8th, 2019. The AM and PM peak hour for each intersection was used to determine the volumes for this analysis. The peak hour volumes used for each intersection are shown below in Figure 1-A.

Figure 1-A: Existing Intersection Peak Hour Volumes



Operating conditions at intersections are evaluated in terms of Level of Service (LOS). LOS A through D is generally considered to be adequate peak hour operations. LOS E and F are generally considered inadequate conditions. However, in urban areas, LOS E is also generally considered acceptable.

Levels of Service for signalized intersections are reported in composite fashion, i.e., one LOS for the entire intersection, and are based on average control delay. Individual turning movements at a signalized intersection may experience inadequate LOS, particularly where these volumes are relatively low, while the intersection as a whole has an adequate LOS. This is because the major movements on the major roadway are given priority in assigning signal green time.

The Highway Capacity Manual Level of Service criteria for signalized intersections are shown in Table 2-A below.

Table 2-A: HCM Intersection Level of Service Criteria

Level-of-Service Criteria Signalized Intersections	
Level-of-Service	Stopped Delay per Vehicle (sec)
A	≤ 10
B	>10 and ≤ 20
C	>20 and ≤ 35
D	>35 and ≤ 55
E	>55 and ≤ 80
F	>80

Table 3-A shows the LOS, the overall intersection delay (in seconds per vehicle) for the study intersections during the 2019 existing conditions.

Table 3-A: Existing Conditions Capacity Analysis Summary

LOS/Delay		2019	
Intersection		Existing	
		AM	PM
Delk Road @ Bentley Road/Woodsmill Drive	Overall	D (48.1)	D (48.8)
	EB	B (19.9)	C (21.4)
	WB	D (42.0)	D (47.4)
	NB	F (135.7)	F (159.4)
	SB	E (63.9)	E (57.4)
Delk Road @ Franklin Gateway	Overall	E (79.2)	E (58.2)
	EB	E (60.1)	D (50.2)
	WB	F (91.9)	E (70.6)
	NB	F (148.8)	D (40.2)
	SB	D (45.7)	D (50.9)
Terrell Mill Road @ Bentley Road	Overall	C (25.2)	D (41.7)
	EB	B (13.5)	C (20.3)
	WB	C (28.5)	E (55.2)
	NB	-	D (52.0)
	SB	D (50.5)	D (51.7)

Legend:	
X (X)	LOS (Delay)
-	No Vehicles Recorded

The intersection of Delk Road & Bentley Road/Woodsmill Drive and Terrell Mill Road & Bentley Road operate at an overall LOS D or better during both peak hours. It should be noted though that the northbound approach of the Delk Road & Bentley Road operates at LOS F during both peak hours.

The intersection of Delk Road & Franklin Gateway operates at LOS E during both peak hours with the westbound and northbound approaches operating at LOS F during the AM peak hour.

Attachment 7

S I & A Report(s)

- a. A soils summary and description
- b. Detailed Geotechnical study including borings will included in preliminary engineering study



May 19, 2020

Stantec

229 Peachtree Street NE
Suite 1900
Atlanta, GA 30303

Attention: Mr. William G. Wise, Jr., RLA, LEED AP
Senior Landscape Architect

Reference: **OMAT File Review**
Rottenwood Creek Bike Trail
Cobb County, Georgia
Contour Project No: G19STA02

Dear Will:

Contour Engineering, LLC (Contour) has completed file review at the Office of Materials and Testing (OMAT) for the subject site referenced above in general accordance with the scope of services outlined in Contour's Cost Proposal Number: E19STA-218. The following letter report provides the results of our review.

PROJECT DESCRIPTION

It is our understanding that the proposed project is to construct a pedestrian bridge over Interstate 75, approximately 700 feet south of the Delk Road underpass. The project is in the early stages of development; therefore, no other information is available at this time about the proposed pedestrian bridge. The purpose of the file review was to determine general subsurface conditions in the vicinity of project and provide preliminary foundation recommendations.

FILE REVIEW

Contour performed a file at the OMAT facility. Files reviewed by Contour included Bridge Foundation Reports for the Delk Road Underpass (boring logs only), Windy Hill Overpass, and Terrell Mill Road Underpass. Summary of each report is as follows:

Delk Road Underpass (boring logs only)

Based on review of the boring logs, weathered rock was typically encountered at elevations ranging from 943 to 922 feet with rock encountered at elevations ranging from 937 to 893 feet. We anticipate that the current bridge is supported on H-piles with tip elevations extended down to weathered rock/rock.

Terrell Mill Road Underpass

Review of the BFI indicates that the intermediate bents are supported on spread footings with bearing elevations ranging from 925 to 927 feet. It appears the spread footings are bearing on weathered rock. Bent #1 appears to be supported on H-piles with an estimated tip elevation of 905 feet. Bent #4 appears to be supported on spread footings with bearing elevation of 936 feet.

Windy Hill Overpass

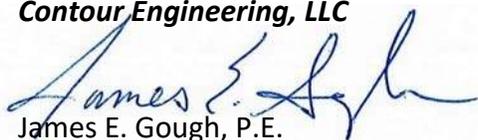
Review of the BFI indicates that all the intermediate bents are supported on H-piles with an estimated tip elevation ranging from 903 to 944 feet. These estimated tip elevations typically coincide with the top of rock.

CONCLUSIONS

Based on our review of the available BFI reports, we anticipate that the proposed pedestrian bridge will be supported on H-piles that will extend to weathered rock/rock elevations. We have attached the reviewed BFI's to this report.

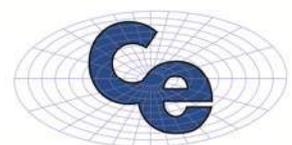
We appreciate the opportunity to work with you on this project and look forward in assisting you with any future projects. Should you have any questions regarding this report or if we may be of further service, please contact our office.

Sincerely,
Contour Engineering, LLC

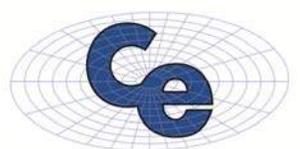


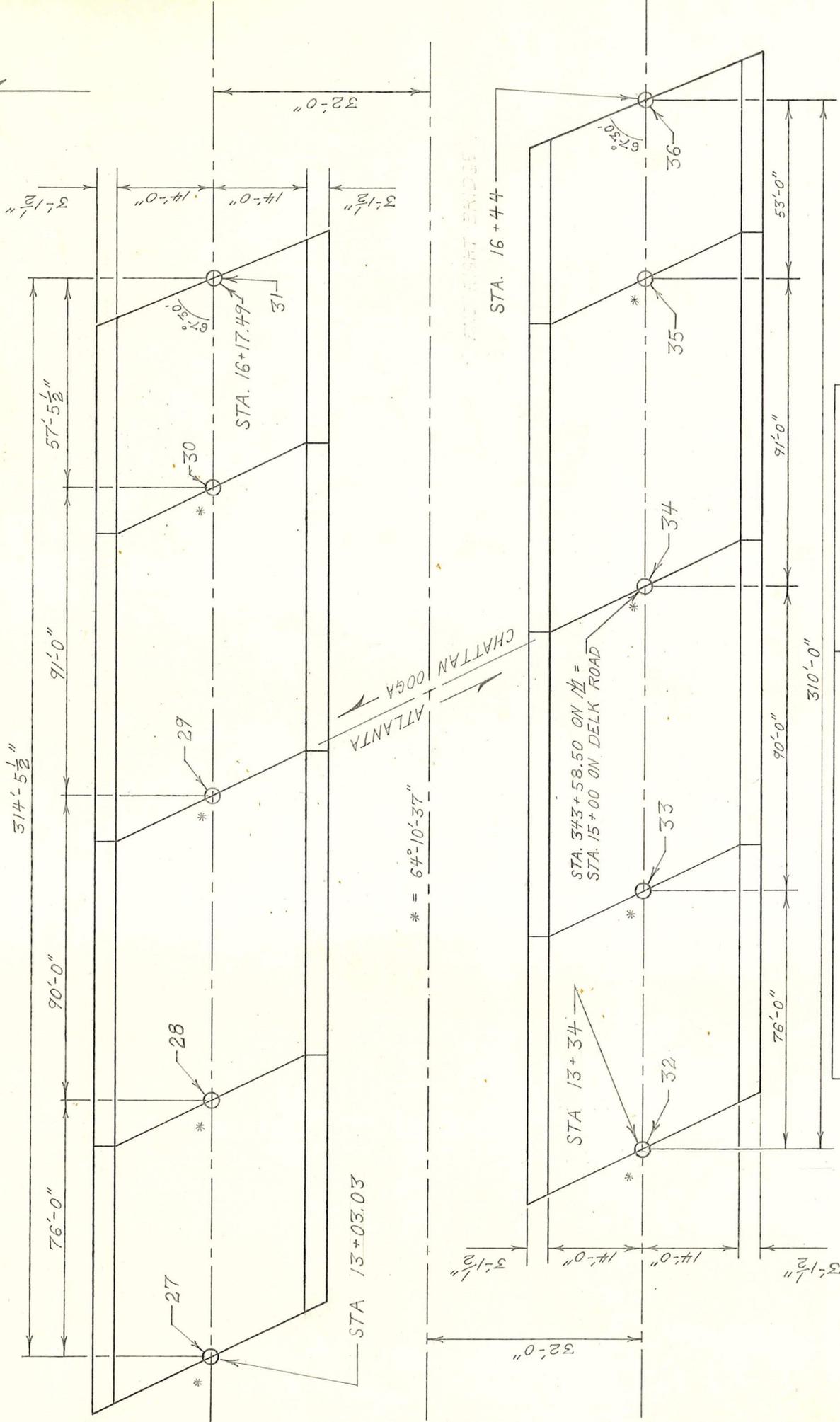
James E. Gough, P.E.
Principal Engineer

Attachments: Delk Road Underpass (boring logs only)
Terrell Mill Road Underpass BFI
Windy Hill Overpass BFI



ATTACHMENTS





State Highway Department of Georgia Bridge Foundation Investigation		I75-3(3)270 Cobb Co. Sta. 343+58.50 Delk Road Underpass No Scale	
DRW. <i>R.S.S.</i>	CKD <i>R.S.S.</i>	APP. TDM	

STATE HIGHWAY DEPARTMENT OF GEORGIA
 DIVISION OF TESTS AND INVESTIGATIONS
 ATLANTA, GEORGIA

DATE 8-16-61

PARTY CHIEF M. MICHEN

REPORT OF BRIDGE SOUNDING INVESTIGATION

PROJECT NO. I-75-3(3)270 COUNTY COBB

LOCATION DELK RD. UNDERPASS

BENT NO. 1 FOOTING LEFT BORING NO. 27

GROUND ELEV. 967.1 PROPOSED FOOTING ELEV. _____

ELEV.	BORING LOG	SAM- PLE	BLOWS	W	γ	Gs	C	φ	EC	AL	e	S	% 200	% CLAY	REMARKS
	GROUND ELEV.														
	RED CLAY & MICA	1-S	21												
960	BROWN SAP. & MICA	2-S	13												
		3-S	10												
	W.L. BROWN SANDY														
950	SAPROLITE & MICA	4-S	18												
	DECOM. GRANITE	5-S	45												
940	FELDSPAR & QUARTZ	6-S	83												
	DECOM. GRANITE														MED. HARD DRILLING
930															
	DECOM. GRANITE														
920	& QUARTZ														HARD DRILLING
910															
900															STOPPED DRILLING AUGERS HANGING

STATE HIGHWAY DEPARTMENT OF GEORGIA
 DIVISION OF TESTS AND INVESTIGATIONS
 ATLANTA, GEORGIA

DATE 8-16-61

PARTY CHIEF POPE

REPORT OF BRIDGE SOUNDING INVESTIGATION

PROJECT NO. I-75-3(3) COUNTY COBB

LOCATION DELK-ROAD UNDERPASS 4' OFFSET

BENT NO. 2 FOOTING LEFT BORING NO. 28

GROUND ELEV. 965.6 PROPOSED FOOTING ELEV. 947.00

ELEV.	BORING LOG	SAM- PLE	BLOWS	W	γ	Gs	C	φ	EC	AL	e	S	% 200	% CLAY	REMARKS
	<u>GROUND ELEV.</u>														
	<u>RED CLAY &</u>														
<u>960</u>	<u>MICA</u>														
	<u>BROWN & WHITE</u>														
	<u>SAPROLITE &</u>														
<u>950</u>	<u>MICA.</u>														
		<u>1-U</u>													
		<u>1-S</u>	<u>43</u>												
		<u>2-S</u>	<u>41</u>												
<u>940</u>	<u>DECOMPOSED</u>														
	<u>GRANITE</u>														
		<u>3-S</u>	<u>35</u>												
		<u>4-S</u>	<u>33</u>												
<u>930</u>		<u>5-S</u>	<u>70=6"</u>												
		<u>6-S</u>	<u>68</u>												
<u>920</u>	<u>RECOM. GRANITE</u>														
	<u>FELDSPAR & QUARTZ</u>														
		<u>7-S</u>	<u>55'=6"</u>												
	<u>DECOMPOSED</u>														
<u>910</u>	<u>GRANITE</u>														
	<u>REFUSAL</u>														
<u>900</u>															

MED. HARD
 DRILLING
 HARD
 DRILLING

STATE HIGHWAY DEPARTMENT OF GEORGIA
 DIVISION OF TESTS AND INVESTIGATIONS
 ATLANTA, GEORGIA

DATE 8-17-61

PARTY CHIEF POPE

REPORT OF BRIDGE SOUNDING INVESTIGATION

PROJECT NO. I-75-3(3)270 COUNTY COBB

LOCATION DELK ROAD UNDERPASS

BENT NO. 4 FOOTING RIGHT BORING NO. 35

GROUND ELEV. 954.4 PROPOSED FOOTING ELEV. 947.0

ELEV.	BORING LOG	SAM- PLE	BLOWS	W	γ	Gs	C	φ	DC	AL	e	S	% 200	% CLAY	REMARKS
	GROUND ELEV.														
950	BROWN SAP. & MICA														
	DECOMPOSED GRANITE FELDSPAR & QUARTZ	1-S	8												
940	DECOMPOSED GRANITE & FELDSPAR	2-S	21												
	DECOM. GRANITE FELDSPAR - SAPROLITE & MICA	3-S	53												
930	DECOMPOSED GRANITE	4-S	63												
		5-S	70 = 6"												MED. HARD DRILLING
920	GRANITE	6-S	81 = 6"												HARD DRILLING
		7-S	150 = 6"												
910	REFUSAL STOPPED DRILLING														

W.L.

STATE HIGHWAY DEPARTMENT OF GEORGIA
 DIVISION OF TESTS AND INVESTIGATIONS
 ATLANTA, GEORGIA

DATE 8-14-61

PARTY CHIEF POPE

REPORT OF BRIDGE SOUNDING INVESTIGATION

PROJECT NO. I-75-3(3) COUNTY COBB

LOCATION DELK ROAD UNDERPASS

BENT NO. 4 FOOTING LEFT BORING NO. 30

GROUND ELEV. 954.7 PROPOSED FOOTING ELEV. 947.0

ELEV.	BORING LOG	SAM- PLE	BLOWS	W	γ	G _s	C	φ	EC	AL	e	S	% 200	% CLAY	REMARKS
	GROUND ELEV.														
950	BROWN SAPROLITE & MICA	1-S	13												
	DECOM. GRANITE SAP. & MICA	1-U													
940	DECOM. GRANITE FELDSPAR & QUARTZ	2-S	15												
	DECOMPOSED GRANITE FELDSPAR & QUARTZ	2-U 3-S 3-U	28												
930	QUARTZ	4-S	63												
	DECOM. SAP. &	5-S	30												
920	MICA														
910	SOFT & HARD LAYERS OF ROCK														
900	DECOM. GRANITE SAPROLITE														
890	REFUSAL														

NO SAMPLE

STATE HIGHWAY DEPARTMENT OF GEORGIA
 DIVISION OF TESTS AND INVESTIGATIONS
 ATLANTA, GEORGIA

DATE 8-11-61

PARTY CHIEF POPE

REPORT OF BRIDGE SOUNDING INVESTIGATION

PROJECT NO. I-75-3(3)270 COUNTY COBB

LOCATION DELK ROAD UNDERPASS

BENT NO. 5 FOOTING CENTERLINE BORING NO. 31

GROUND ELEV. 951.60 PROPOSED FOOTING ELEV. AIR

ELEV.	BORING LOG	SAM- PLE	BLOWS	W	γ	G _s	C	φ	BC	AL	e	S	% 200	% CLAY	REMARKS
	GROUND ELEV.														
950	BROWN SAP. & MICA	1-S	8												
	BROWN SILTY SAP. & MICA.	2-S	12												
940	BROWN SILTY SAP. & MICA.	3-S	13												
	WITH FELDSPAR & QUARTZ	4-S	10												
930		5-S	45												
	BROWN SAP. MICA														
920	SOFT-HARD ROCK														
910	REFUSAL														
900															

DRILLING
WITH 500
LB. OF PRES.

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

FILE IR-75-3(168) COBB COUNTY OFFICE Materials and Research
Forest Park, Georgia
DATE February 23, 1988

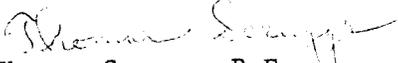
FROM David A. Mitchell, Chief, Geotechnical Engineering Bureau

TO Charles Lewis, State Bridge and Structural Design Engineer

SUBJECT Bridge Foundation Investigation
I-75 o/Terrell Mill Road (widening)

As requested, a bridge foundation investigation has been made at the above listed site. The results of this work are attached.

If any additional information is needed, please advise.


Thomas Scruggs, P.E.
Engineering Design Unit

TS :cdj

Attachment

copy: Paul Mullins

BRIDGE FOUNDATION INVESTIGATION

PROJECT NO.: IR-75-3(168) COBB COUNTY

LOCATION: See Map I-75 over Terrell Mill Road (widening)

GEOLOGIC FORMATION: Biotite Gneiss/Mica Schist/Amphibolite Formation

NOTABLE SUBSURFACE FEATURES: Rock near elevation 907 at northbound bridge, and near elevation 917 at southbound bridge.

MAXIMUM PILE DESIGN LOADS

TIMBER =	12" MS=	12" PSC=	10 BP42 = 55 tons
END BEARING-(X)	14" MS=	14" PSC=	12 BP 53= 70 tons
	16" MS=	16" PSC=	
FRICITION- ()	18" MS=	18" PSC=	14 BP 73= 96 tons

FOUNDATION RECOMMENDATIONS

BENT	SPREAD FTG. (BEARING)	PILE FTG. (PILE TYPE)	PILE BENT (PILE TYPE)
1	()	()	(H)
2,3	(7 ksf)	on weathered rock()	()
4	(5 ksf)	on dense soil ()	()
	()	()	()
	()	()	()

ELEVATIONS

BENT	BOTTOM of FTG.	MINIMUM TIP	ESTIMATED TIP
<u>Northbound Bridge</u>			
Right side			
1		913	905 - 910
2,3	902 or below		
4	925 or below		
<u>Left side</u>			
1		913	905 - 910
2,3	905 or below		
4	927 or below		
<u>Southbound Bridge</u>			
1		913	905 - 910
2,3	912 or below		
4	936 or below		

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy. No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate. This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

P.D.O. - Driving resistance after minimum tip elevations have been achieved.

Spread Footings - Footings should be poured as soon as possible after excavations are complete. Excavations should be protected from rainfall or surface runoff. A minimum 2 feet cover should be provided on top of footings. Excavation in very hard material will be required at some intermediate bents in rock or dense saprolite during footing excavation. Some light blasting of rock may also be required.

Retaining Wall - A tieback wall was proposed at bent 1. We recommend the following soil parameters for design use:

Soil Unit Weight	$\gamma = 120$ pcf
Cohesion	$c = 100$ pcf
Angle of Internal Friction	$\phi = 24^\circ$

continued

BRIDGE FOUNDATION INVESTIGATION

ADDITIONAL INFORMATION (CONTINUED)

IR-75-3(168) COBB COUNTY
I-75 over Terrell Mill Road (widening)

Retaining Wall (continued) - Estimated bond stress will be 1-3 kips/ft., and will be dependent on the method of installation. During excavation for the tieback wall, temporary bracing such as the use of soil nailing or other methods will be required to stabilize the fill at bent 1.

A minimum clearance of 2 feet between the back face of the tieback wall and the H-piles has been recommended to avoid transmitting movements in the wall to the piles during stressing of the anchors.

Boulders and/or rock layers were encountered during drilling at bent 1; these may interfere with installation of the anchors, and the Contractor should be made aware of these conditions.

Waiting Period - A 30 day waiting period is recommended before driving piles through new fills at bent 1.

Balken Piles - Due to the presence of boulders in the fills at bent 1, Balken piles are not recommended for use as an alternate.

Additional Information - A request was made to locate the tops of the existing footings at bents 2 and 3. Due to existing concrete slope paving at these bents, however, we were unable to accomplish this.

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy. No claim will be considered if the contractor or relies on this information in his bidding or in his construction operations and finds that it is inaccurate.

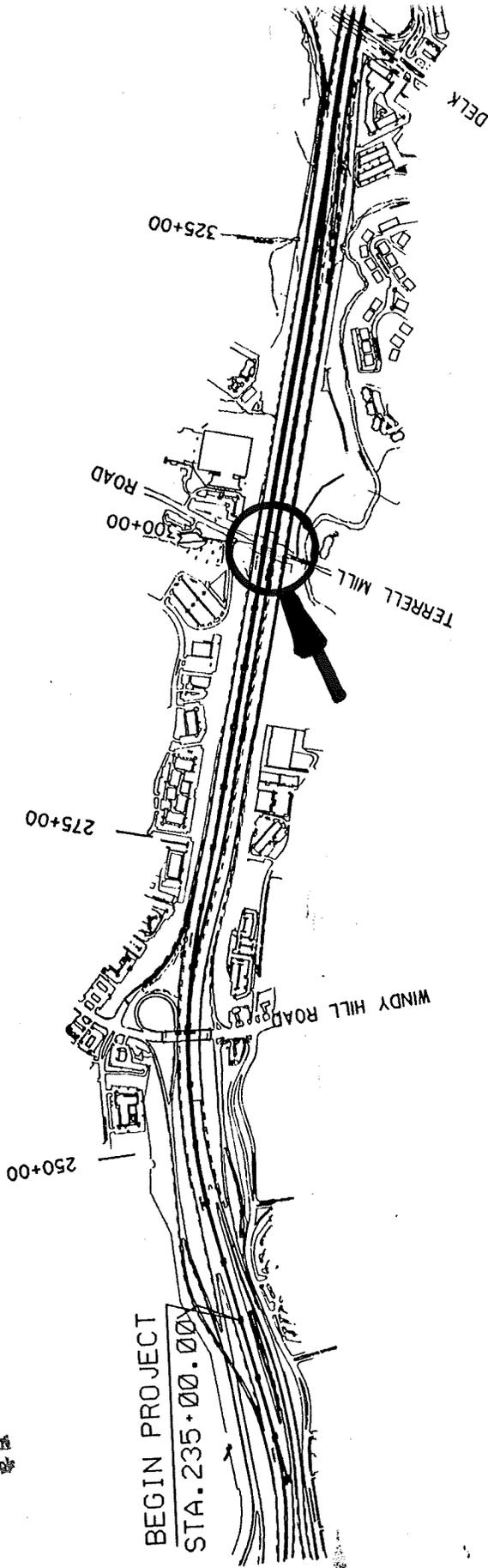
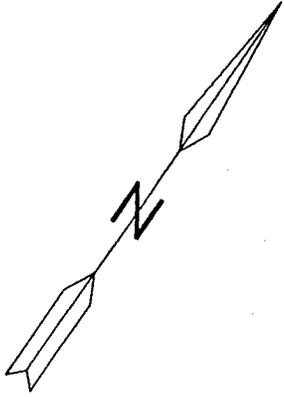
This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

Reviewed by: Waverly Bailey, P.E.

Reported By: Thomas Scruggs

Drafted By: Claire Hollis

PLAN AND PRO I-75 FROM I-2 COE IR-



The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy. No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate. This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DESIGN DATA:
TRAFFIC A.D.T.
TRAFFIC A.D.T.
TRAFFIC D.H.V.

LOCATION	SKETCH
----------	--------

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 2 / 2 / 88
 LOCATION I-75 O/TERRELL MILL RD. BR. NO. 1 LT. & RT. BORING NO. 3
 BENT NO. 1 FOOTING 88' RT. C STA. 299+45 SBL GROUND ELEV. 929.19
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ		
930	Gr. Elev.													
920	Med. Dse. Mltc. Micas Sdy. Silt (Fill) 1s	13												
	2s	16												
	Dse. Mltc. Micas Sdy. Silt 3s	28												
910	Rock Lyrs.													
	Refusal													
900														
890														
880														
870														

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy. No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate. This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 2 / 2 / 88
 LOCATION I-75 O/TERRELL MILL RD. BR. NO. I LT. & RT. BORING NO. 5
 BENT NO. 1 FOOTING 129' RT. C STA. 299+50 SBL GROUND ELEV. 928.39
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ
930	Gr. Elev.											
	Boulders & Fill											
920												
	Med. Dse. Brn. Micas Sdy. Silt	1s										
		2s										
910												
	V. Dse. Gr. & White Micas Sdy. Silt	3s										
		60=3'										
900	Refusal											
890												
880												
870												

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.
 No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate.
 This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 1/29/88
 LOCATION I-75 O/TERRELL MILL RD. BR. NO. 1 LT. & RT. BORING NO. 7
 BENT NO. 2 FOOTING 54' RT. C SBL GROUND ELEV. 916.18
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ		
	Gr. Elev.													
	Boulders & Rock													
	Refusal													
910														
900														
890														
880														
870														
860														
850														

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy. No claim will be considered if the contractor or relies on this information in his bidding or in his construction operations and finds that it is inaccurate. This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 1/29/88

LOCATION I-75 O/TERRELL MILL RD. BR. NO. I LT. & RT. BORING NO. 8

BENT NO. 2 FOOTING 74' RT. $\frac{1}{2}$ SBL GROUND ELEV. 915.00

PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ
915.00	Gr. Elev.											
910.00	Boulders & Rock											
910.00	Refusal											
900.00												
890.00												
880.00												
870.00												
860.00												
850.00												

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.

No claim will be considered if the contractor or relies on this information in his bidding or in his construction operations and finds that it is inaccurate.

This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT.2 COUNTY COBB DATE 1/29/88
 LOCATION I-75 O/TERRELL MILL RD. BR. NO. 1 LT. & RT. BORING NO. 9
 BENT NO. 2 FOOTING 94' RT. $\frac{1}{2}$ SBL GROUND ELEV. 914.21
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ
910	Gr. Elev. \nearrow Boulders & Rock Refusal \curvearrowright											
900												
890												
880												
870												
860												
850												

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy. No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate. This foundation investigation report is not considered as a part of the Plans and Specifications of Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 1/29/88
 LOCATION I-75 O/TERRELL MILL RD. BR. NO. 1 LT. & RT. BORING NO. 10
 BENT NO. 2 FOOTING 114' RT. ϵ SBL GROUND ELEV. 912.69
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ
910	Gr. Elev. \curvearrowright Boulders & Rock											
900	Refusal \curvearrowright											
890												
880												
870												
860												
850												

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.
 No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate.
 This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 1/29/88
 LOCATION I-75 O/TERRELL MILL RD. BR. NO. I LT. & RT. BORING NO. 11
 BENT NO. 2 FOOTING 134' RT. C SBL GROUND ELEV. 910.45
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ		
910	Gr. Elev.													
	Boulders & Rock													
	Refusal													
900														
890														
880														
870														
860														
850														

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.

No claim will be considered if the contractor or relies on this information in his bidding or in his construction operations and finds that it is inaccurate.

This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 1/29/88

LOCATION I-75 O/TERRELL MILL RD. BR. NO. 1 LT. & RT. BORING NO. 12

BENT NO. _____ FOOTING 255' RT. $\frac{1}{2}$ SBL GROUND ELEV. 906.49

PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ
	Gr. Elev.											
	Gravel & Boulders											
900	Refusal											
890												
880												
870												
860												
850												
840												

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.

No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate.

This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 2 / 1 / 88
 LOCATION I-75 O/TERRELL MILL RD. BR. NO. 1 LT. & RT. BORING NO. 13
 BENT NO. 3 FOOTING 54' RT. E SBL GROUND ELEV. 916.62
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ
	Gr. Elev.											
	Boulders & Rock											
910	Refusal											
900												
890												
880												
870												
860												
850												

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy. No claim will be considered if the contractor relies on this information in his bidding or his construction operations and finds that is inaccurate. This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 2/1/88

LOCATION I-75 O/TERRELL MILL RD. BR. NO. I LT. & RT. BORING NO. 14

BENT NO. 3 FOOTING 84' RT. $\frac{1}{2}$ SBL GROUND ELEV. 913.85

PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ		
910	Gr. Elev.  Boulders & Rock													
900	Refusal 													
890														
880														
870														
860														
850														

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy. No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate. This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 2 / 1 / 88
 LOCATION I-75 O/TERRELL MILL RD. BR. NO. 1 LT. & RT. BORING NO. 15
 BENT NO. 3 FOOTING 114' RT. C SBL GROUND ELEV. 912.66
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ	
910	Gr. Elev. \curvearrowright Boulders & Rock												
900	Refusal \curvearrowright												
890													
880													
870													
860													
850													

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy. No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate. This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 2/1/88
 LOCATION I-75 O/TERRELL MILL RD. BR. NO. 1 LT. & RT. BORING NO. 16
 BENT NO. 3 FOOTING 134' RT. E SBL GROUND ELEV. 910.48
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORINBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ	
910	Gr. Elev. ↗												
	Boulders, Gravel, Sand												
	Refusal ↗												
900													
890													
880													
870													
860													
850													

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.

No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate.

This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 2 / 1 / 88
 LOCATION I-75 O/TERRELL MILL RD. BR. NO. 1 LT. & RT. BORING NO. 17
 BENT NO. 3 FOOTING 255' RT. $\frac{1}{2}$ SBL GROUND ELEV. 905.82
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ	
	Gr. Elev.												
	Boulders & Backfill												
900	Refusal												
890													
880													
870													
860													
850													
840													

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy. No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate. This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 2/2/88

LOCATION I-75 O/TERRELL MILL RD. BR. NO. 1 LT. & RT. BORING NO. 18

BENT NO. 4 FOOTING 55' RT. C SBL STA. 301+20 GROUND ELEV. 944.09

PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ		
	Gr. Elev.													
940	Med. Dse. Brn. Sdy. Silt													
	Dse. Brn. Sdy. Silt	45	Is											
930		29	2s											
	Rock													
	Refusal													
920														
910														
900														
890														
880														

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.

No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate.

This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT IR-75-3(168) CT. 2 COUNTY COBB DATE 2 / 2 / 88

LOCATION I-75 O/TERRELL MILL RD. BR. NO. I LT. & RT. BORING NO. 20

BENT NO. 4 FOOTING 130' RT. E STA. 300 + 80± GROUND ELEV. 932.56

PROPOSED FOOTING ELEV. _____ PARTY CHIEF SPORNBERGER

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	φ
930	Gr. Elev. ↗											
	Weathered Rock Lys.											
920	Refusal on Hard Rock ↗											
910												
900												
890												
880												
870												

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy. No claim will be considered if the contractor or relies on this information in his bidding or if his construction operations and finds that it is inaccurate. This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

LEGEND OF SYMBOLS

Sample	u (undisturbed) s (split spoon)
Blows	number of blows with a 140 pound hammer dropping 30 in. required to drive split spoon 1 ft.
W	Water content (%)
γ	Unit weight of undisturbed sample (pcf)
G_s	Specific gravity of soils solids
e	Void ratio
S	Degree of saturation (%)
LL	Liquid limit (%)
PI	Plasticity Index (%)
C	Cohesion - apparent or real (ksf)
ϕ	Angle of internal friction (degrees)
q	Indicated ultimate strength (ksf)
B.C.	Estimated safe bearing capacity based on assumptions noted on report sheet (ksf)
<u>wl</u>	Water level at time boring was made
NP	Non-plastic
NR	No results or no recovery

Current Wall Number	Approximate Stations and Location	Previous Wall Number	Original Project Number
A	420+00 to 430+00, Right (1+42 to 10+90, Ramp D)	A	IR-75-3 (168), Cobb
B	446+00 to 459+00, Right	B B	IR-75-3 (168) and IR-75-3 (170)
C	N.B. I-75 at South SR 120	-	IR-75-3 (164)
E	460+00 to 461+09, Right	E	IR-75-3 (164)
A-1	298+00 to 298+50, Right	A-1	IR-75-3 (168)
B-1	324+80 to 329+40, Left (34+43 to 29+83, Ramp E)	B-1	IR-75-3 (168)
	-Wall B-2 has been eliminated -	-	IR-75-3 (168)
C-1	396+93 to 417+73, Left	C	IR-75-3 (168)
RW-2A	95+00 to 96+60, Right	2	PEM-9000 (21) Cobb
RW-2B	98+22 to 100+66, Right	2	PEM-9000 (21)
RW-3	66+40 to 69+25, Right	3	PEM-9000 (21)
	- Wall 1 has been eliminated -	-	PEM-9000 (21)
A-2	I-75 at Terrell Mill Road	-	IR-75-3 (168)
D	453+00 to 456+32, Left	A	IR-75-3 (170)
	Walls C, D and E from Project IR-75-3 (170) are not in this contract -		
1	156+97 to 162+35, Right (SR 120)	1	ERF-114-1 (63), Cobb
R	11+61 to 13+25, Left (SR 120)	R	IR-75-3 (168)
	- Wall 4 from project PEIR-75-3 (165) has been eliminated -		

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

FILE PEI-75-3 (84) Cobb County OFFICE Materials & Research
Forest Park, Georgia
DATE August 29, 1978

FROM David A. Mitchell, Chief, Geotechnical Engineering Bureau

TO John T. Kratzer, State Bridge & Structural Design Engineer

SUBJECT Bridge Foundation Investigation
Windy Hill Road over I-75

As requested, a bridge foundation investigation has been made at the above listed site. The results of this work are attached.

If any additional information is needed, please notify us.

DAM: bfs

Enclosure

copy: Larry G. Adams

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.

No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate.

This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

BRIDGE FOUNDATION INVESTIGATION

PROJECT NO.: PEI-75-3 (84) Cobb County
 LOCATION: See Map Windy Hill Road over I-75

GEOLOGIC FORMATION: Biotite Gneiss and Schist
 NOTABLE SUBSURFACE FEATURES: Irregularly weathered rock (dipping from right to left) overlain by loose to dense residual soil.

MAXIMUM PILE DESIGN LOADS

TIMBER =	12" MS =	14" PSC =	10 BP 42 = 55 tons
END BEARING - (85%)	14" MS =	16" PSC =	12 BP 53 = 70 tons
FRICITION - (15%)	16" MS =	18" PSC =	14 BP 73 = 90 tons

FOUNDATION RECOMMENDATIONS

<u>BENT</u>	<u>SPREAD FTG. (BEARING)</u>	<u>PILE FTG. (PILE TYPE)</u>	<u>PILE BENT (PILE TYPE)</u>
ALL	()	("H")	("H")
	()	()	()
	()	()	()
	()	()	()
	()	()	()

ELEVATIONS

<u>BENT</u>	<u>BOTTOM of FTG.</u>	<u>MINIMUM TIP</u>	<u>ESTIMATED TIP</u>
1		940	903 Left to 933 Right
2		930	905 Left to 927 Right
3		930	915 Left to 920 Right
4		945	936 Left to 944 Right

PDO - Driving resistance after minimum tip elevation is achieved.

Alternate Design - Most footings at Bents 2 & 3 may be spread at or below proposed footing elevations using a maximum safe design pressure of 2.5 TSF. However, footings at Bent 3 left should not be spread.

End Fill Settlement - End rolls should be allowed to settle for 30 days before driving end bent piles only.

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.

No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate.

This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

Reported By: David A. Mitchell

Drafted By: Steve Hand

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT PEI-75-3(84) COUNTY COBB DATE 8/23/78
 LOCATION WINDY HILL RD. RELOC. 0/1-75 BORING NO. 1
 BENT NO. 1 FOOTING 41' Lt. C GROUND ELEV. 957.63
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF HOLLIS

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ
	Gr. El. <u>7</u>											
	Medium Dense Mltc.											
950	Micas. Sandy Silt	1s	13									
	Same W/ Wood	2s	16									
		3s	14									
940		4s	16									
	Medium Dense Mltc.	5s	15									
930	Micas. Sandy Silt	6s	19									
		7s	19									
920		8s	46									
	Dense Same (W.R.)	9s	27									
910		10s	33									
	Very Hard Rock											
900	Refusal in Rock											

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.

No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate.

This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT PEI-75-3(84) COUNTY COBB DATE 8/27/78
 LOCATION WINDY HILL RD. RELOC. 0/1-75 BORING NO. 3
 BENT NO. 1 FOOTING 35' Rt. ϕ Offset 3' Bk. GROUND ELEV. 967.64
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF HOLLIS

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ	
	Gr. El. <u>?</u>												
960	Loose Mltc. Micas. Sandy Silt	1s 6											
	Very Dense Same	2s 60=1'											
950	Medium Dense Mltc. Micas Sandy Silt	3s 23											
	Dense Same (W.R.)	4s 41											
940	Very Dense Mltc. Micas. Sandy Silt (W.R.)	5s 60=5' 6s 52											
930	Very Hard Rock Refusal in Rock	7s 60=0'											

The Department of Transportation in making this report available to contractors assumes no responsibility for its accuracy.

No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate.

This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT PEI-75-3(84) COUNTY COBB DATE 8/23/78
 LOCATION WINDY HILL RD. RELOC. 0/1-75 BORING NO. 4
 BENT NO. 2 FOOTING 41' Lt. ϕ Offset 3' Bk. GROUND ELEV. 945.49
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF HOLLIS

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ		
	Gr. El. \uparrow													
940	Dense Mltc. Micas. Sandy Silt	1s 27												
		2s 15												
930	Medium Dense Same	3s 13												
		4s 16												
920		5s 54												
		6s 60												
910	Very Dense Mltc. Micas. Sandy Silt (W.R.)	7s 60												
		8s 60=7'												
	End Drilling \uparrow													

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.

No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate.

This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT PEI-75-3(84) COUNTY COBB DATE 8/22/78
 LOCATION WINDY HILL RD. RELOC. 0/1-75 BORING NO. 5
 BENT NO. 2 FOOTING 15' Lt. ϕ GROUND ELEV. 944.84
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF HOLLIS

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ		
	Gr. El. <u>7</u>													
940	Medium Dense Mltc. Micas. Sandy Silt	1s												
		2s												
930		3s												
Gwt	Dense Same (W.R.)	4s												
920		5s												
	V. Dense Mltc. Micas. Sandy Silt (W.R.)	6s	60=7'											
910		7s	60=1'											
	Very Hard Rock													
	Refusal in Rock <u>↗</u>													

The Department of Transportation in making this investigation report available to contractors assumes no responsibility for its accuracy.

No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate.

This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT PEI-75-3(84) COUNTY COBB DATE 8/22/78
 LOCATION WINDY HILL RD. RELOC. 0/1-75 BORING NO. 6
 BENT NO. 2 FOOTING 6'Rt. ϕ Offset 3' Bk. GROUND ELEV. 944.63
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF HOLLIS

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ		
	Gr. El. <u>7</u>													
940	Medium Dense Mltc Micas. Sandy Silt	1s 20												
	Dense Same	2s 34												
930	V. Dense Mltc. Micas. Sandy Silt (W.R.)	3s 60=6'												
Gwt.	Medium Dense Mltc. Micas. Sandy Silt	4s 15												
920		5s 60=6'												
	Very Dense Same (W.R.)	6s 60												
910		7s 60=7'												
	Very Hard Rock													
	Refusal in Rock <u>↗</u>													

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.
 No claim will be considered if the contractor or relies on this information in his bidding or in his construction operations and finds that it is inaccurate.
 This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA

SOILS ENGINEERING AND GEOLOGY BRANCH

BRIDGE SUBSURFACE INVESTIGATION

PROJECT PEM-004 (4) COUNTY COBB DATE 8/8/78
 LOCATION WINDY HILL RD. RELOC. O/I-75 BORING NO. 9
 BENT NO. 3 FOOTING 15' LT. C 5' BK. GROUND ELEV. 946.66
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF HOLLIS

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	φ
	GR. EL. ↘											
940	Loose Mltc Micac Sandy Silt W/Grvl.	1s 10										
	Dense Mltc Micac Sandy Silt	2s 45										
930	Med. Dense Same	3s 15										
	Dense Same	4s 37										
920		5s 25										
	V. Dense Same W/Weath. Rock	6s 60 = .5'										
	V. Dense Weath. Rock											
910	Refusal in Rock ↗											
900												

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.

No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate.

This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA

SOILS ENGINEERING AND GEOLOGY BRANCH

BRIDGE SUBSURFACE INVESTIGATION

PROJECT PEM-004 (4) COUNTY COBB DATE 8/10/78
 LOCATION WINDY HILL RD. RELOC. O/I-75 BORING NO. 11
 BENT NO. 3 FOOTING 32' RT. C 4' BK. GROUND ELEV. 945.77
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF HOLLIS

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ
	GR. EL. <u>945.77</u>											
940	Dense Mltc Micas Sandy Silt	1s 43										
930	V. Dense Same W/ Weath. Rock	2s 60 = .7'										
		3s 60 = .2'										
	Dense Mltc Micas Sandy Silt	4s 37										
920	V. Dense Same W/ Weath. Rock V. Dense Rock	5s HB										
	Refusal in Rock											
910												

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy. No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that it is inaccurate. This foundation investigation report is not considered as a part of the specifications or contract documents.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT PEI-75-3(84) COUNTY COBB DATE 8/1/78
 LOCATION WINDY HILL RD. RELOC. 0/1-75 BORING NO. 12
 BENT NO. 4 FOOTING 41' Lt. C Offset 48' Bk. GROUND ELEV. 946.83
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF HOLLIS

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ		
	Gr. El. <u>7</u>													
940	Medium Dense Mltc. Micas. Sandy Silt	1s 18												
	Very Dense Same	2s 60=7'												
930	very hard rock	3s 60=7'												
	Refusal in Rock													

This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT PEI-75-3(84) COUNTY COBB DATE 8/21/78
 LOCATION WINDY HILL RD. RELOC. 0/1-75 BORING NO. 13
 BENT NO. 4 FOOTING 11' Lt. ϕ Offset 44' Bk. GROUND ELEV. 950.09
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF HOLLIS

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ		
950	Gr. El. \uparrow													
Gwt.	V. Dense Mltc. Micas. Sandy Silt (W.R.)	1s												
940	Dense Same	2s												
		3s												
930	Very Hard Rock													
	Refusal in Rock \nearrow													

The Department of Transportation hereby makes this foundation report available to contractors but assumes no responsibility for its accuracy.

No claim will be considered if the contractor relies on this report for design or construction in the contract documents.

This report is not to be considered as a part of the contract and specifications or Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT PEI-75-3(84) COUNTY COBB DATE 8/21/78
 LOCATION WINDY HILL RD. RELOC. 0/1-75 BORING NO. 15
 BENT NO. 4 FOOTING 31' Rt. $\frac{1}{2}$ Offset 33' Bk. GROUND ELEV. 958.04
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF HOLLIS

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ		
960	Gr. El. \uparrow													
950	Very Dense Mltc. Micas. Sandy Silt (W.R.)	1s 60=7'												
		2s 60=7'												
		3s 60=.1'												
940	Very Hard Rock Refusal in Rock \nearrow													

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy.

No claim will be considered if the contractor relies on this information in his bidding or in his construction operations and finds that the rock is not as shown.

This foundation investigation report is not to be used as a part of the Plans and Specifications of any Contract on the job.

DEPARTMENT OF TRANSPORTATION

OFFICE OF MATERIALS AND RESEARCH, FOREST PARK, GEORGIA
 GEOTECHNICAL ENGINEERING

BRIDGE SUBSURFACE INVESTIGATION

PROJECT PEI-75-3(84) COUNTY COBB DATE 8/21/78
 LOCATION WINDY HILL RD. RELOC. 0/1-75 BORING NO. 18
 BENT NO. 4 FOOTING 32' Rt. ϕ Offset 12' Bk. GROUND ELEV. 958.84
 PROPOSED FOOTING ELEV. _____ PARTY CHIEF HOLLIS

ELEV.	BORING LOG	BLOW	UNIFIED	γ	W	Gs	% 200	% CLAY	LL	PI	C	ϕ		
960	Gr. El. \nearrow													
	Medium Dense Mltc. Micas. Sandy Silt	1s 23												
950	Very Dense Same (W.R.)	2s 57												
		3s 60=1'												
940	Very Hard Rock													
	Refusal in Rock \nearrow													

The Department of Transportation in making this foundation report available to contractors assumes no responsibility for its accuracy. No claim will be considered if the contractor or relies on this information in his bidding or in his construction operations and finds that it is inaccurate. This foundation investigation report is not considered as a part of the Plans and Specifications or Contract on the job.

Attachment 8

MS4 Concept Report Summary

- a. MS4 Concept Report Summary
- b. MS4 Drainage Area Layout

**MS4 Concept Report for
Rottenwood Creek Trail Phase II
City of Marietta, Cobb County, Georgia**

MS4 CONCEPT REPORT



Prepared for:
Office of Program Delivery
Georgia Department of Transportation
600 West Peachtree Street, 25th Floor
Atlanta, GA 30308

Prepared by:
Stantec Consulting Services, Inc.
229 Peachtree Street
International Tower, Suite 1900
Atlanta, GA 30303

November 25, 2019

**MS4 CONCEPT REPORT FOR
ROTTENWOOD CREEK TRAIL PHASE II
CITY OF MARIETTA, COBB COUNTY, GEORGIA**

Table of Contents

1.0	EXECUTIVE SUMMARY	1
2.0	PROJECT DESCRIPTION AND CONCLUSIONS	1
2.1	APPENDIX A - VINCINITY MAPS / PROJECT MAP	4
2.2	APPENDIX B – MS4 CONCEPT REPORT SUMMARY	5
2.3	APPENDIX C – DRAINAGE AREA MAP	6
2.4	APPENDIX D – CURVE NUMBER CN CALCULATION.....	7
2.5	APPENDIX E – GDOT_MS4_TOOL_WORKSHEET	8
2.6	APPENDIX F – NRCS – CUSTOM SOILS REPORT AND MAP	9

**MS4 CONCEPT REPORT FOR
ROTTENWOOD CREEK TRAIL PHASE II
CITY OF MARIETTA, COBB COUNTY, GEORGIA**

November 25, 2019

1.0 EXECUTIVE SUMMARY

According to Municipal Separate Storm Sewer System (MS4) Permit (General NPDES Permit No. GAR041000) issued by the Environmental Protection division (EPD) of the Georgia Department of Natural Resources, the new and existing point source discharges of stormwater from roadways owned and operated by GDOT to water of the State of Georgia are regulated and the proposed infrastructure project must meet the requirements of the Permit, which include incorporating permanent water quality control and detention measures (best management practices [BMPs]) into the design where appropriate, and where those BMPs have not been determined to be infeasible based on the exclusion and infeasibility criteria identified in Section 1.2 and Section 1.4 of the GDOT Guidelines for Design of Post-Construction BMPs issued June 12, 2014.

Upon analyzing the Project area, we have concluded that three out of five outfalls within project area qualify for the Outfall Level Exclusion No.6. This exclusion states “Road projects that has reduction (or negligible increase) in impervious area are excluded”, as defined by GDOT. One out of five outfalls within project area qualifies for the Outfall Level Exclusion No.4. This exclusion states “Stormwater discharges exit R/W as sheet flow are excluded”, as defined by GDOT. Only one out of five outfalls within project area does not qualify for any of Level Exclusions and the post stormwater BMPs will be needed to meet water quality requirement for this outfall.

2.0 PROJECT DESCRIPTION AND CONCLUSIONS

The proposed project will add a 10' wide concrete shared-use path along a 1.24-mile extent of Franklin Gateway crossing over Delk Road, then along Delk Road following the I-75 on-ramp, crossing I-75 via proposed pedestrian bridge, south along the right-of-way of I-75 to Rottenwood Creek, along Rottenwood Creek back to Delk Road and along Delk Road to Bentley Road. The site consists of approx. 7.5 acres (from the face of curb to right of way on new trail side). The proposed construction will consist of adding new 10' concrete shared-use path, crosswalk, construction of ADA compliant ramps and driveways, etc. Due to the characteristics of this project such as only adding sidewalk, not adding new curbs, not altering road alignment and all travel lanes are intact, the overall proposed construction contributing to the increased stormwater runoff from the existing right-of-way (ROW) will be insignificant.

Per MS4 Concept stormwater requirements, the determination of items deemed applicable to project can be excluded from the project entirely if one of the “Project Level Exclusions” is met. If a project does not meet a Project Level Exclusion, specific outfall drainage areas within a project may meet an “Outfall Level Exclusion”. The site was analyzed using the provided survey as well as USGS quad map. Upon analyzing the Project area, detailed findings are as follow:

**MS4 CONCEPT REPORT FOR
 ROTTENWOOD CREEK TRAIL PHASE II
 CITY OF MARIETTA, COBB COUNTY, GEORGIA**

November 25, 2019

1. The total project site area is approx. 7.5 acres and the total disturbed area is approx. 2.3 acres.
2. Drainage patterns for both existing and proposed conditions are similar since the most of proposed grades are to matching existing grades. There are five drainage areas with five corresponding outfalls within project limit.
 - Drainage Area 1 - Outfall 1: Stormwater from proposed shared path flows towards the existing road at max. slope of 2.0% and converges with the rest of drainage area 1 runoff, then flows southeast to the outfall 1 at Rottenwood Creek via sheet and gutter flow and existing storm piping system.
 - Drainage Area 2 - Outfall 2: Stormwater from proposed shared path flows towards the existing road at max. slope of 2.0% and converges with the rest of drainage area 2 runoff, then flows north to the outfall 2 at Rottenwood Creek via sheet and gutter flow and existing storm piping system.
 - Drainage Area 3 - Outfall 3: Stormwater from proposed shared path flows towards the existing road at max. slope of 2.0% and converges with the rest of drainage area 3 runoff, then flows east and then southeast to the outfall 3 at Rottenwood Creek via sheet and gutter flow and existing storm piping system.
 - Drainage Area 4 - Outfall 4: Stormwater from proposed shared path flows southeast towards the existing Rottenwood Creek along with the rest of drainage 4 runoff, then to the outfall 4 at Rottenwood Creek via sheet and gutter flow.
 - Drainage Area 5 - Outfall 5: Stormwater from proposed shared path flows towards the existing road at max. slope of 2.0% and converges with the rest of drainage area 5 runoff, then flows east to the outfall 5 at Rottenwood Creek via sheet and gutter flow.

See Appendix 2.3

3. The proposed impervious areas for each outfall have been calculated. See summary chart below:

IMPERVIOUS AREA SUMMARY

Outfall	Existing Impervious Areas (sq ft)	Proposed Impervious Areas (sq ft)	Percentage Increased %
1	191344	193494	1.12%
2	208135	214160	2.89%
3	373633	394173	5.50%
4	126932	142492	12.26%
5	33873	36998	9.23%
Total	933917	981317	5.08%

**MS4 CONCEPT REPORT FOR
ROTTENWOOD CREEK TRAIL PHASE II
CITY OF MARIETTA, COBB COUNTY, GEORGIA**

November 25, 2019

See Appendix 2.3

4. This proposed shared path in drainage area 3 is along the bank of Rottenwood Creek. The vegetated buffers will be acting as a BMP for post stormwater.
5. Since there are no significant increase in peak flows for 1 yr, 25 yr, 50 yr, 100 yr storm event between post-development and pre-development, therefore the channel, overbank flood and extreme flood protection measures will not be necessary. In addition, the impact to downstream will be negligible as well.

Therefore, we have reached the following conclusions:

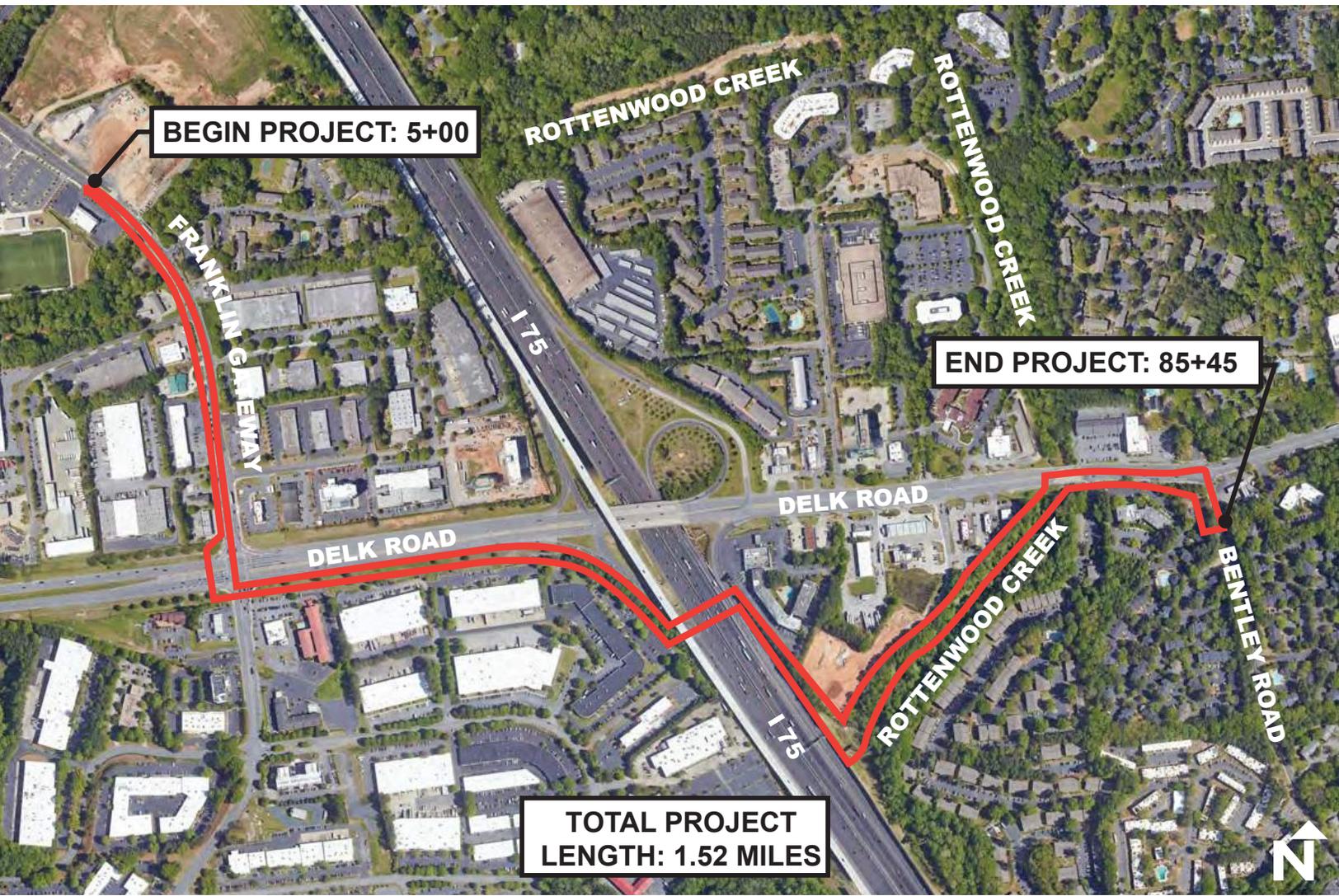
- Outfall No. 1 : qualifies for the Outfall Level Exclusion No.6
- Outfall No. 2 : qualifies for the Outfall Level Exclusion No.6
- Outfall No. 3 : Not qualify for the Outfall Level Exclusions. Filter Strip and Bioslope post stormwater BMPs will be implemented during design process.
- Outfall No. 4 : qualifies for the Outfall Level Exclusion No.4
- Outfall No. 5 : qualifies for the Outfall Level Exclusion No.6

See Appendix 2.5 – GDOT_MS4_TOOL_WORKSHEET

**MS4 CONCEPT REPORT FOR
ROTTENWOOD TRAIL
CITY OF MARIETTA, COBB COUNTY, GEORGIA**

November 25, 2019

2.1 APPENDIX A - VINCINITY MAPS / PROJECT MAP



BEGIN PROJECT: 5+00

END PROJECT: 85+45

**TOTAL PROJECT
LENGTH: 1.52 MILES**

FRANKLIN GATEWAY

DELK ROAD

ROTTENWOOD CREEK

ROTTENWOOD CREEK

DELK ROAD

I 75

I 75

ROTTENWOOD CREEK

BENTLEY ROAD



**MS4 CONCEPT REPORT FOR
ROTTENWOOD TRAIL
CITY OF MARIETTA, COBB COUNTY, GEORGIA**

November 25, 2019

2.2 APPENDIX B – MS4 CONCEPT REPORT SUMMARY

MS4 Concept Report Summary

Attach the following checklist information to the Concept Report Template:

Is there a Project Level Exclusion that applies to this project: No Yes

If yes, please indicate which of the following exclusions apply:

- Roadways that are not owned or operated (maintained) by GDOT may not require post-construction BMPs. Coordinate with the appropriate local government or entity to determine stormwater management requirements.
- The project location is not within a designated MS4 area.
- Maintenance and safety improvement projects whereby the sites are not connected and disturbs less than one acre at each individual site. This includes projects such as repaving, shoulder building, fiber optic line installation, sign addition, and sound barrier installation.
- Projects that have their environmental documents approved or right-of-way plans submitted for approval on or before June 30th, 2012.
- Road projects that disturb less than 1 acre or for site development projects that add less than 5,000 ft² of impervious area.

If the project has a Project Level Exclusion nothing further is needed.

If the project does not have a Project Level Exclusion use the MS4 Concept Level Design Spreadsheet to estimate the treatment volumes and flow rates, size the BMP's, complete the tables below, and include as an attachment to the Concept Report. Add additional rows, if necessary. It is understood that this information will be approximate based on available information at the time of the concept.

In MS4 designated areas, water quantity requirements may be waived for drainage areas that flow directly into surface waters that have a drainage area greater than 5 square miles.

Drainage Area Summary									
Outfall Area	Pre-Development			Post-Development			Water Quality Volume (Cubic Feet)	Channel Protection Volume (Cubic Feet)	Required Detention Volume (Cubic Feet)
	Tc	Weighted CN	Area (Acres)	Tc	Weighted CN	Area (Acres)			
1	20	92	5.60	20	92	5.60	196	N/A	N/A
2	20	87	7.81	20	87	7.81	549	N/A	N/A
3	20	84	16.98	20	84	16.98	1843	N/A	N/A
4	5	80	7.87	5	81	7.87	1411	N/A	N/A
5	5	85	1.42	5	86	1.42	274	N/A	N/A

BMP Selection and Feasibility Summary						
Outfall Area	Outfall Level Exclusion?		BMP Selected	Is the BMP Feasible?		
	Y/N	Exclusion No.		Y/N	Infeasibility Criteria No.	¹ Feasibility of an Infiltration BMP
1	Y	6				
2	Y	6				
3	N		FS / BS	Y		Well-suitable
4	Y	4				
5	Y	6				

**MS4 CONCEPT REPORT FOR
ROTTENWOOD TRAIL
CITY OF MARIETTA, COBB COUNTY, GEORGIA**

November 25, 2019

2.3 APPENDIX C – DRAINAGE AREA MAP

OVERALL DRAINAGE AREA MAP



PRE-BASIN 1
5.60 ACRES
CN = 92.0
TC = 20 MIN

POST-BASIN 1
5.60 ACRES
CN = 92.0
TC = 20 MIN

PRE-BASIN 2
7.81 ACRES
CN = 87.0
TC = 20 MIN

POST-BASIN 2
7.81 ACRES
CN = 87.0
TC = 20 MIN

PRE-BASIN 3
16.98 ACRES
CN = 84.0
TC = 20 MIN

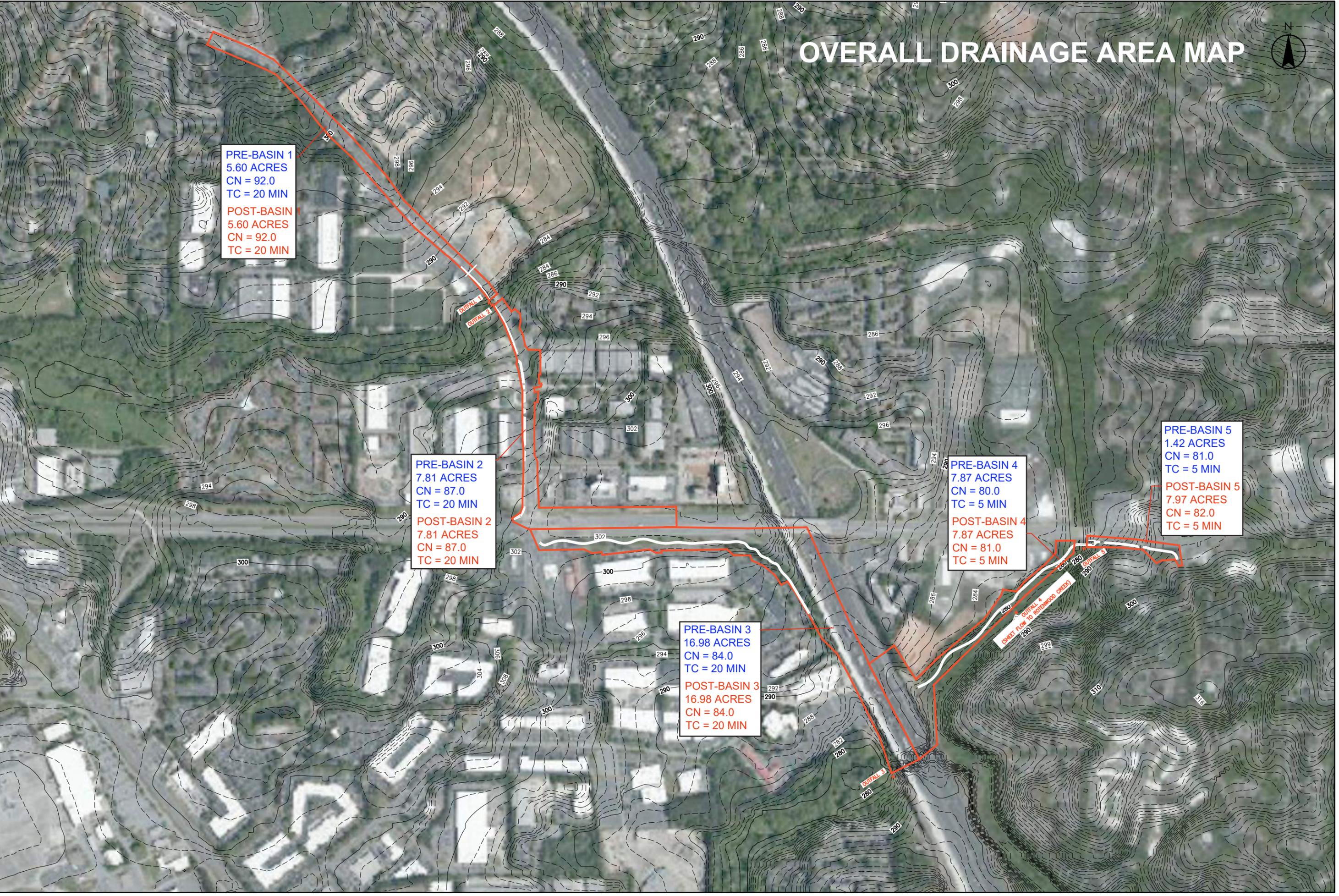
POST-BASIN 3
16.98 ACRES
CN = 84.0
TC = 20 MIN

PRE-BASIN 4
7.87 ACRES
CN = 80.0
TC = 5 MIN

POST-BASIN 4
7.87 ACRES
CN = 81.0
TC = 5 MIN

PRE-BASIN 5
1.42 ACRES
CN = 81.0
TC = 5 MIN

POST-BASIN 5
7.97 ACRES
CN = 82.0
TC = 5 MIN

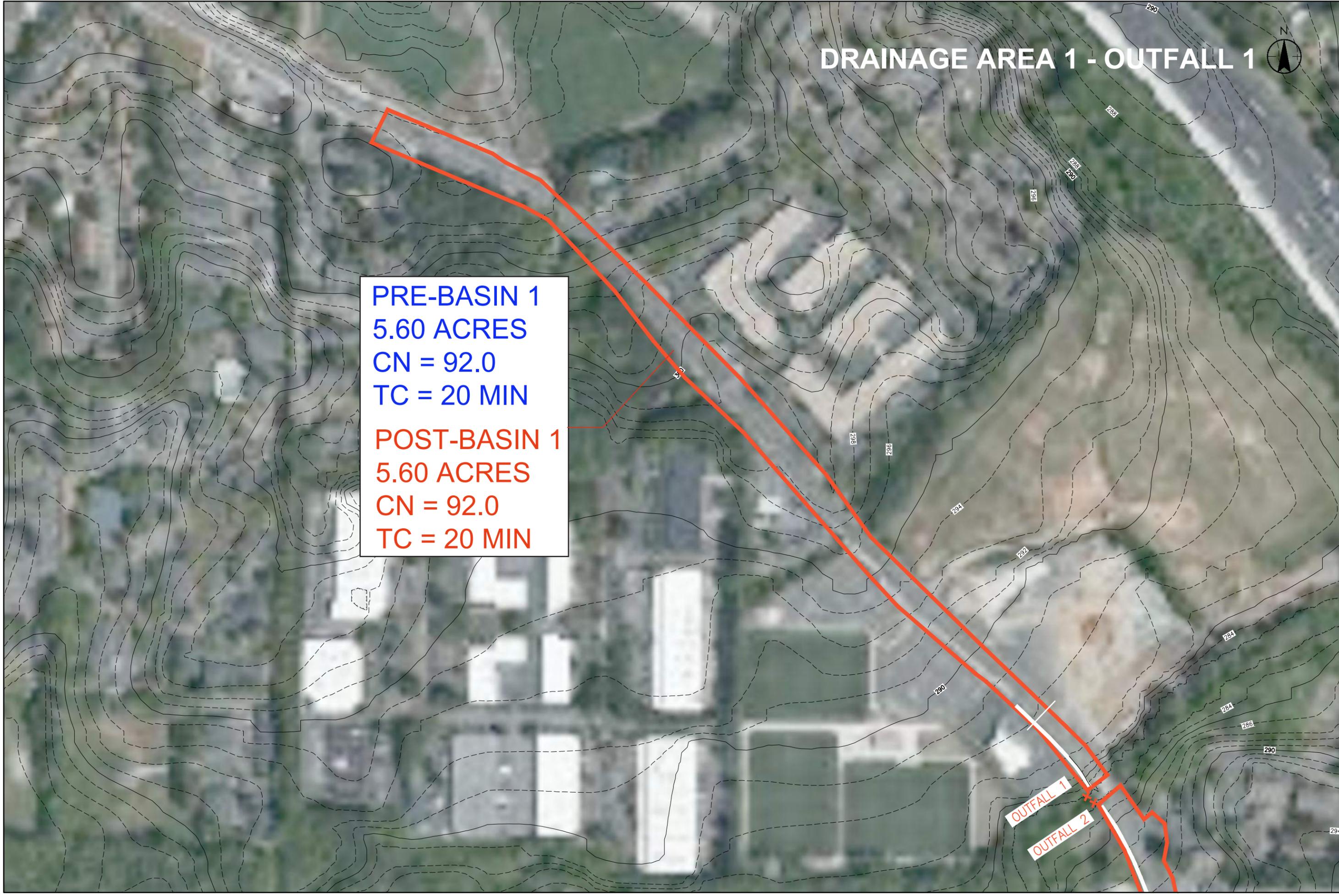


DRAINAGE AREA 1 - OUTFALL 1



PRE-BASIN 1
5.60 ACRES
CN = 92.0
TC = 20 MIN

POST-BASIN 1
5.60 ACRES
CN = 92.0
TC = 20 MIN



DRAINAGE AREA 2 - OUTFALL 2

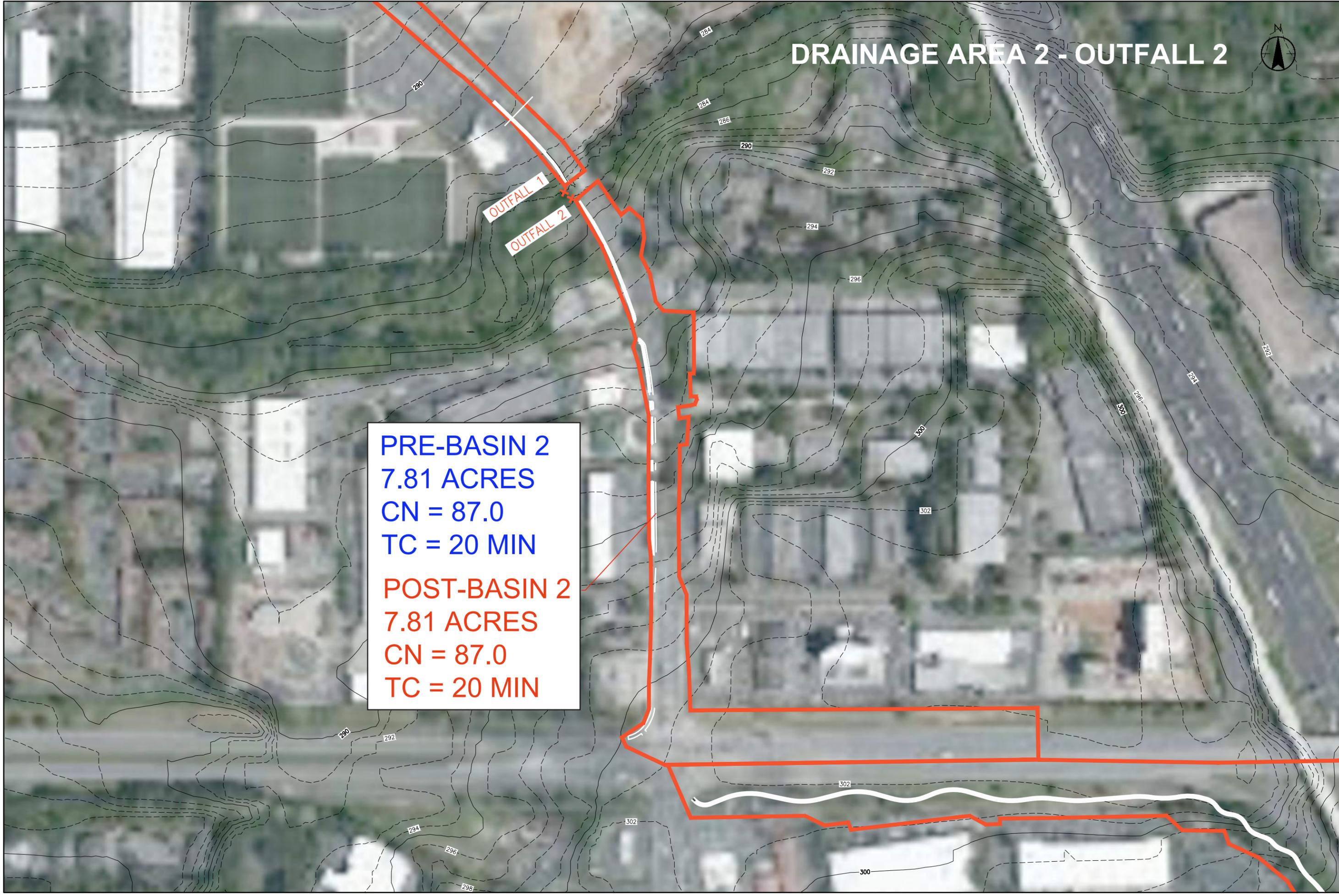


OUTFALL 1

OUTFALL 2

PRE-BASIN 2
7.81 ACRES
CN = 87.0
TC = 20 MIN

POST-BASIN 2
7.81 ACRES
CN = 87.0
TC = 20 MIN



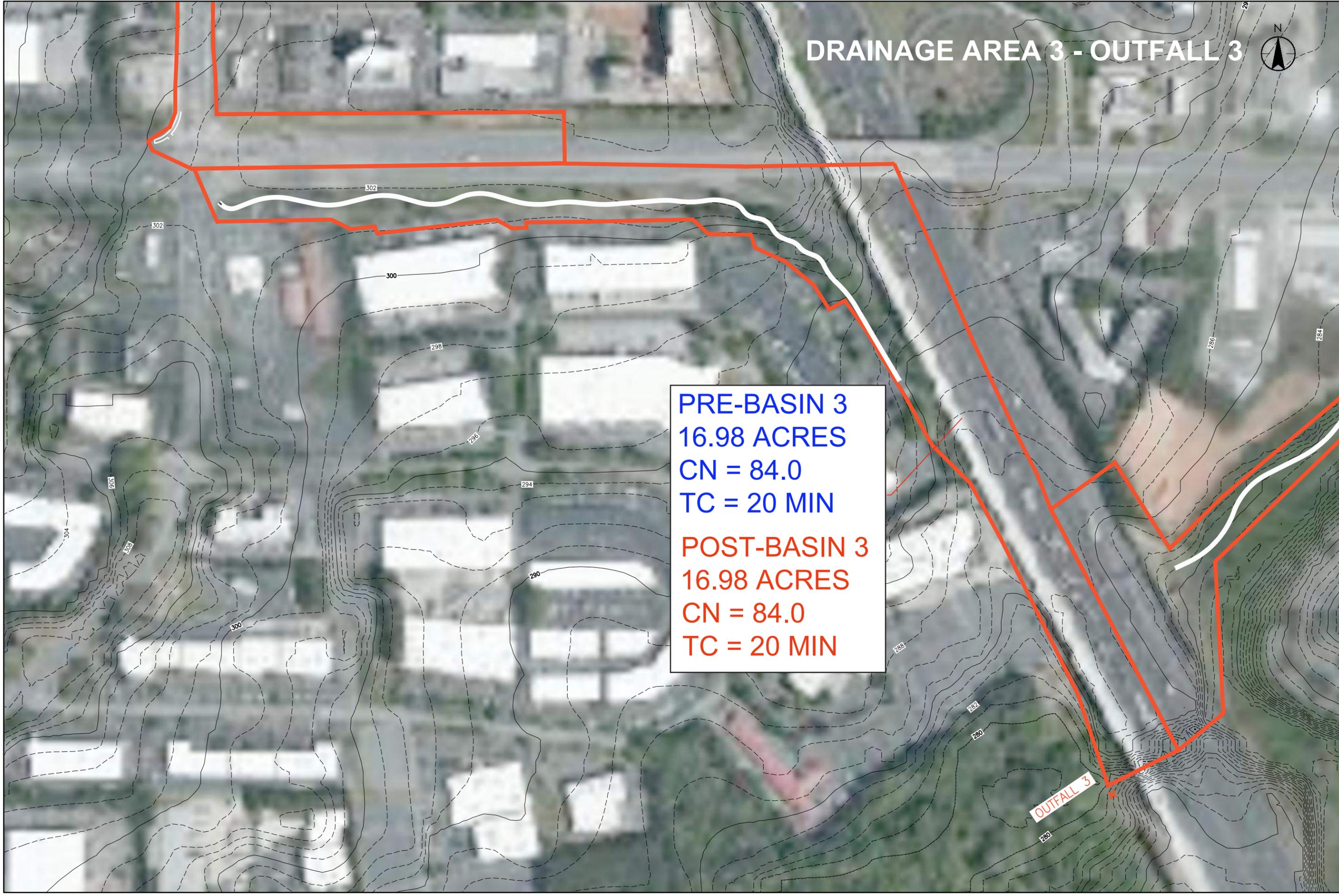
DRAINAGE AREA 3 - OUTFALL 3



PRE-BASIN 3
16.98 ACRES
CN = 84.0
TC = 20 MIN

POST-BASIN 3
16.98 ACRES
CN = 84.0
TC = 20 MIN

OUTFALL 3





PRE-BASIN 4
7.87 ACRES
CN = 80.0
TC = 5 MIN

POST-BASIN 4
7.87 ACRES
CN = 81.0
TC = 5 MIN

PRE-BASIN 5
1.42 ACRES
CN = 81.0
TC = 5 MIN

POST-BASIN 5
7.97 ACRES
CN = 82.0
TC = 5 MIN

PRE-BASIN 3
16.98 ACRES
CN = 84.0
TC = 20 MIN

POST-BASIN 3
16.98 ACRES
CN = 84.0
TC = 20 MIN

OUTFALL 4
(SHEET FLOW TO ROTENWOOD CREEK)

OUTFALL 5

OUTFALL 3

DRAINAGE AREA 4 - OUTFALL 4
DRAINAGE AREA 5 - OUTFALL 5



Attachment 9

Minutes of Concept Meetings

- a. Initial Concept Meeting held 10/17/2019
- b. Concept Meeting held 2/26/2020



Interoffice Memo

FILE: P.I. 0015049, Cobb County
ROTTENWOOD CREEK TRAIL PHASE 11 – SCOPING ONLY

DATE: September 26th, 2019

FROM: Kimberly W. Nesbitt, State Program Delivery Administrator

TO: See Distribution

Jason W. Mobley FOR:

SUBJECT: Initial Concept Team Meeting and Presentation

In accordance with the Plan Development Process, a Concept Team Meeting for the above referenced project has been scheduled for **10:00AM – 12:00PM on Thursday October 17th, 2019** at the **GDOT GO, 4TH Floor RM. 405, 600 West Peachtree Street, Atlanta GA, 30308**. A representative from your office is invited to attend and participate in this meeting.

The purpose of the meeting will be to discuss the need and purpose, logical termini, potential environmental and utility impacts, review alternatives, and to determine the anticipated public involvement.

By copy of this letter, and in accordance with the PDP, we request the District Engineer notify local and state officials; Office of Planning to notify the appropriate MPO, RDC, and/or GRTA representative; Office of Utilities to notify utility company representatives, and the Office of Environmental Services to notify appropriate federal & non-federal resource agencies of the meeting and request their attendance.

Project Materials for your review can be found at the following path on Project Wise at: [PI#0015049\PE\Program Delivery\PM Diary\Draft Documents\Concept\ICTM](#)

Please review this material prior to the meeting and provide written comments to the Project Manager by **(Friday October 11th, 2019 at 5:00PM)** Please note that the initial concept team meeting minutes will include your written comments and the project team's responses.

If you have any questions concerning the project prior to the meeting, please contact the Project Manager, Olusola Adekonojo, at (404)-631-1545 or oadekonojo@dot.ga.gov.

KWN:KESD:JWM:OTA

Attachments:

cc: Hiral Patel, Director of Engineering

Albert V. Shelby, III, Director of Program Delivery
Bobby Hilliard, State Program Control Administrator
Brent Story, State Design Policy & Support Engineer
Attn: Dave Peters, Conceptual Design Group Manager
Andy Casey, State Roadway Design Engineer
Paul Tanner, State Transportation Planning Administrator
Bill Duvall, State Bridge Engineer
Attn: BridgeOffice@dot.ga.gov
Patrick Allen, State Utilities Engineer
OFMConcept@dot.ga.gov
Erik Rohde, State Project Review Engineer
Eric Duff, State Environmental Administrator
RW-ConceptMtgs_Est@dot.ga.gov
John Hancock, State Construction Engineer
Andy Doyle, State Maintenance Engineer
Engsvcsconcepts@dot.ga.gov
Kathy Zahul, District 7 Engineer
Scott Lee, District 7 Construction Manager
Attn: Lee Upkins, Construction Project Manager
Paul DeNard, District 7 Preconstruction Engineer
Attn: Joseph Cavins, District Design Engineer
Justin Hatch, District 7 Traffic Engineer
Shun Pringle, District 7 Utilities Manager
Jean Hee Barrett, Atlanta Regional Commission
via email: jbarrett@atlantaregional.org
Maria Roell, Atlanta Regional Commission
via email: MRoell@atlantaregional.org
Sam Samu, SRTA
via email: (ssamu@srta.ga.gov)
Monica Flournoy, State Materials Engineer
Andrew Heath, State Traffic Engineer
Benny Walden, State Location Bureau Chief
Olusola Adekonojo, Project Manager

Rottenwood Creek Initial Concept Team Meeting
Rottenwood Creek / 178460151

Date/Time: October 17, 2019 / 10:00 AM
Place: GDOT Headquarters Downtown Atlanta
Attendees: GDOT - Olusola Adekonojo, Paul DeNard, Jessica Kern, Ted Hicks, Krystal Stovall-Dixon, Annie Williams, Rachel Perrine, Steve Gaston; City of Marietta – Courtney Verdier, Mark Simmons, Dan Conn; Stantec – Mark Sweeney, Stephen Hopper, Will Wise

1. Stephen went through the history of the project and explained the current direction and justification for that direction
2. Question posed if Franklin Road was on transmission line. Yes.
3. Concept report to be submitted after PIOH and before end of the year.
4. Question about why crossing under I-75 and other alternatives were not used, and will they be included in the concept report. Stephen explained the rationale, and Courtney pointed toward the original study showing the process behind the discarded alternatives.
5. Question about whether a project justification statement had been written, and who is responsible. Stantec will provide.
6. Question about communication with FHA or CRTA concerning ped bridge. CRTA has been included in correspondence up to this point.
7. Discussion was had about a dry run for the PIOH – where and when that would occur.
8. Fact sheet is under way of development. All PIOH materials will be reviewed prior to PIOH.
9. Check list sent for PIOH materials – needs to be submitted to OED?
10. Question about timeline for environmental survey. Potential Phase II elements will be addressed during preliminary design phase.

The meeting adjourned at 11:15 AM

The foregoing is considered to be a true and accurate record of all items discussed. If any discrepancies or inconsistencies are noted, please contact the writer immediately.

Will Wise

SIGN-IN SHEET – February 26, 2020

PROJECT: PI# 0015049, Cobb County- ROTTENWOOD CREEK TRAIL PHASE 11 – SCOPING ONLY - CTM

TIME: 9:00AM – 11:00AM

LOCATION: GDOT GO – 4TH Floor RM. 409, 600 West Peachtree Street, Atlanta, GA 30308

NAME	ORGANIZATION/TITLE	EMAIL ADDRESS	PHONE NO.
Sola Adekonojo	GDOT-OPD/ PM	oadekonojo@dot.ga.gov	(404) 631 1545
Jason Mobley	GDOT –OPD/ DPM	jmobley@dot.ga.gov	(404) 631 1158
MARK SWEENEY	STANTEC	mark.sweeney@stantec.com	(404) 769-6823
Courtney Verdier	City of Marietta	cverdier@mariettaga.gov	(770) 794-5717
Reginald James	ARC	rjames@atlantaregional.org	(470) 378-1438
Jessica Kern	GDOT- Environmental	JKern@dot.ga.gov	(404) 631-1159
Megan Weiss	GDOT- Planning	mweiss@dot.ga.gov	(404) 631-1779
Morgan Bernard	City of Marietta	mbernard@marietta.ga.gov	(770) 794-5648
Mare Simmons	City of Marietta	msimmons@marietta.ga.gov	(770) 792 3958
Krystal Stovall-Dixon	GDOT-OPD/AOH	kstovall-dixon@dot.ga.gov	(404) 631-1572
Wade Woodard	GDOT-D7 Utilities	WWoodard@dot.ga.gov	(404) 216-3916
Willie Simmons	GDOT-D7 Utilities	WSimmons@dot.ga.gov	(404) 216-3974
Shun Pringle	GDOT DT UTH		()
Andrew Pearson	GDOT TRF OPS.		()
Stephen Hopper	STANTEC		()
WILLIAM G. WISE, JR	STANTEC	Will.Wise@stantec.com	(404) 825-5521

Rottenwood Creek Trail Phase 2

GDOT
Concept Team Meeting
February 26, 2020



Project Management Team

Rottenwood Creek Trail Phase II

Courtney Verdier
Daniel Cummings
Jesus Sanchez
Rich Deckman
Dan Conn

City of Marietta Project Manager
City of Marietta
City of Marietta
City of Marietta
City of Marietta

Eric Meyer

Cobb County DOT

Joe Knight

Gateway Marietta CID

Kyethea Clark

Cumberland CID

Olusola Adekonojo

GDOT

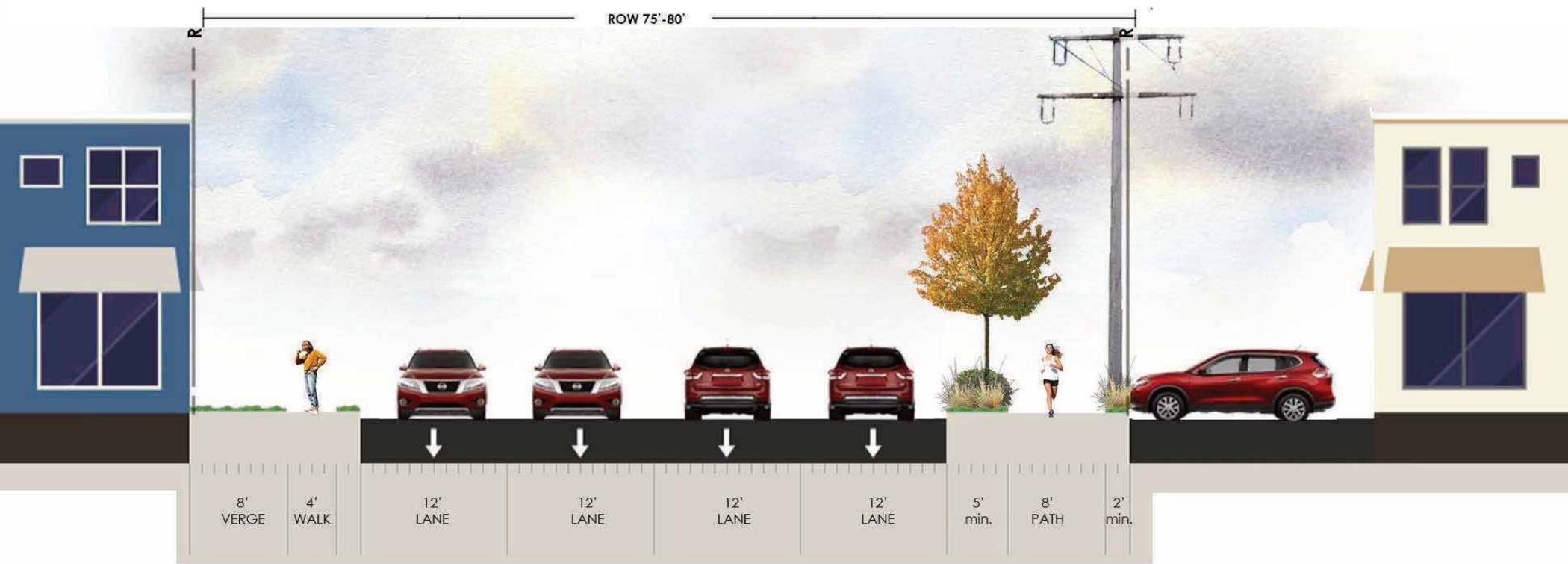
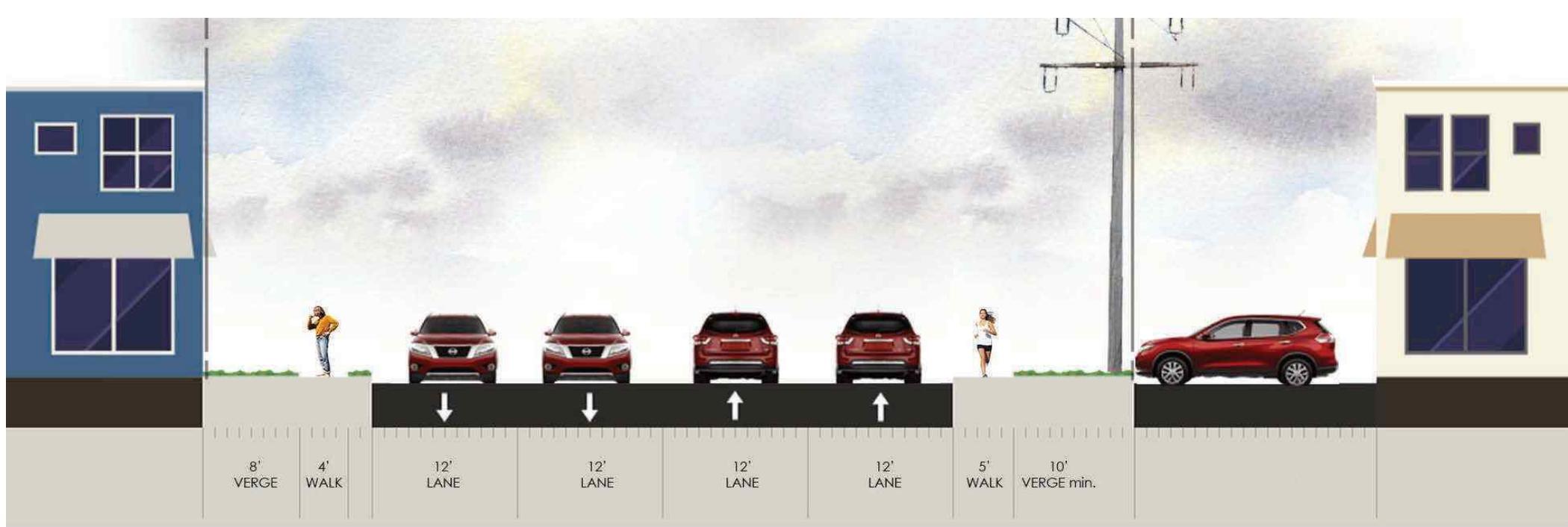
Will Wise
Stephen Hopper
Mark Sweeney
Mitchell Greenway

Stantec
Stantec
Stantec
Stantec



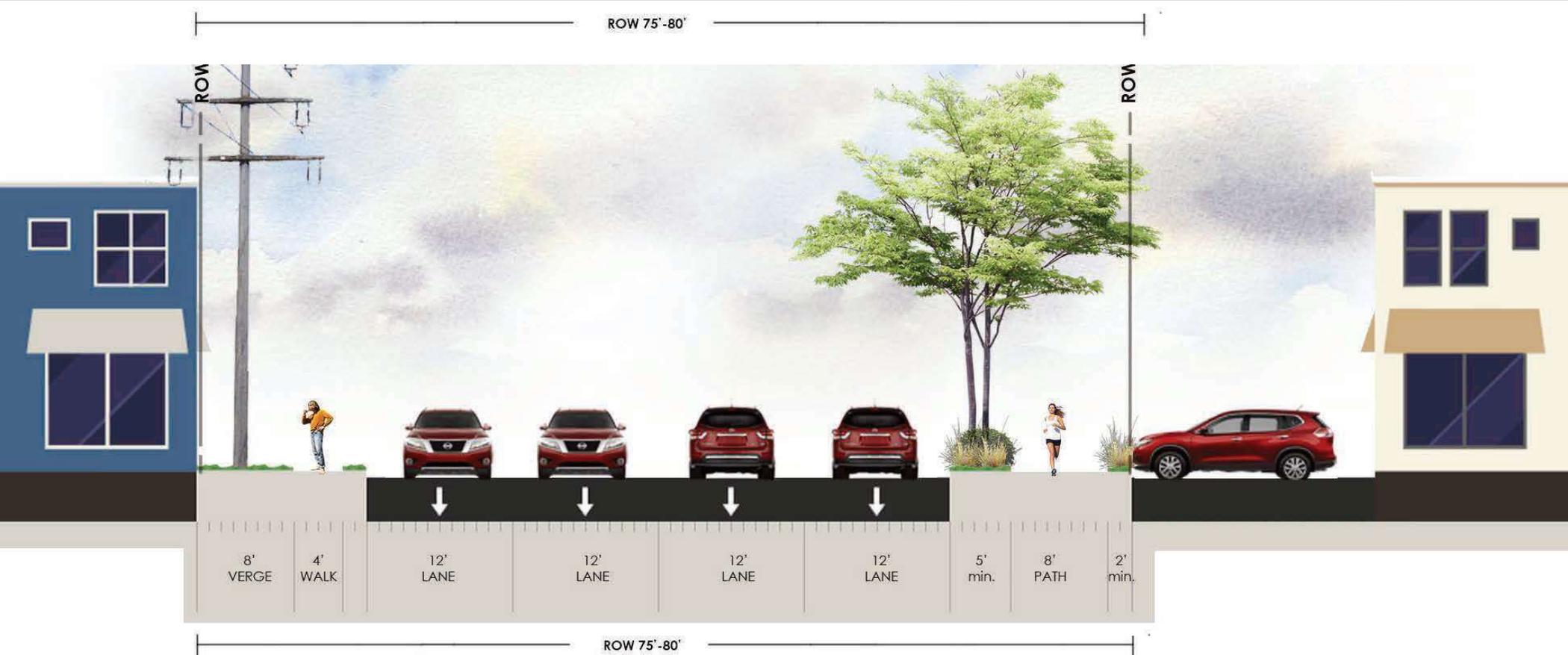
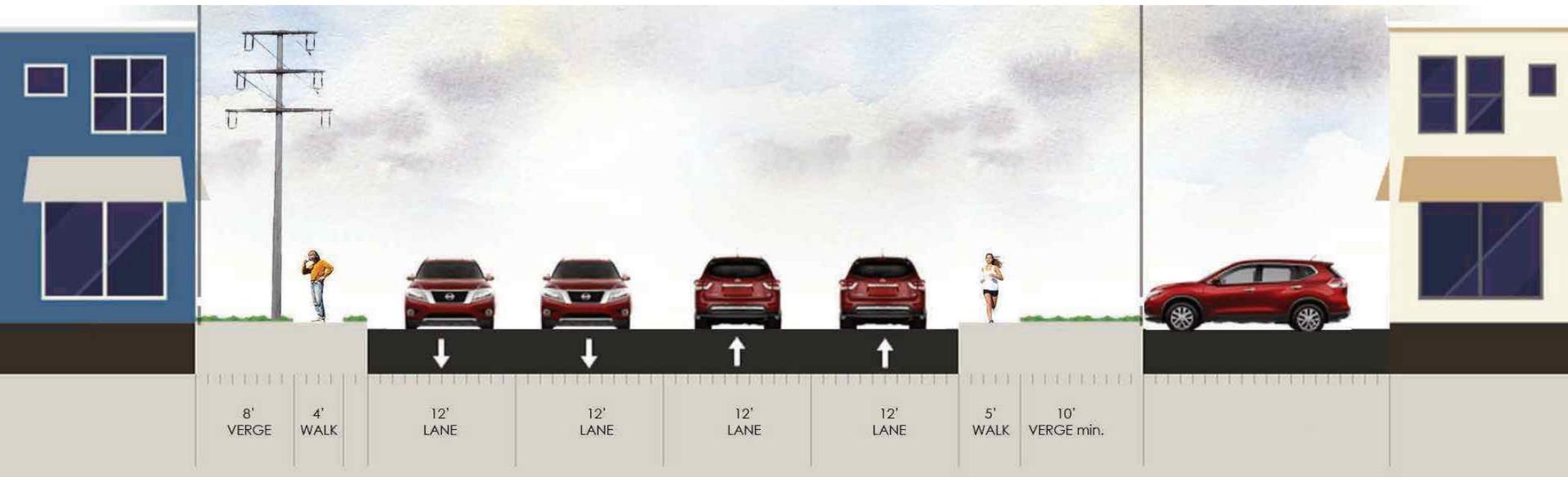


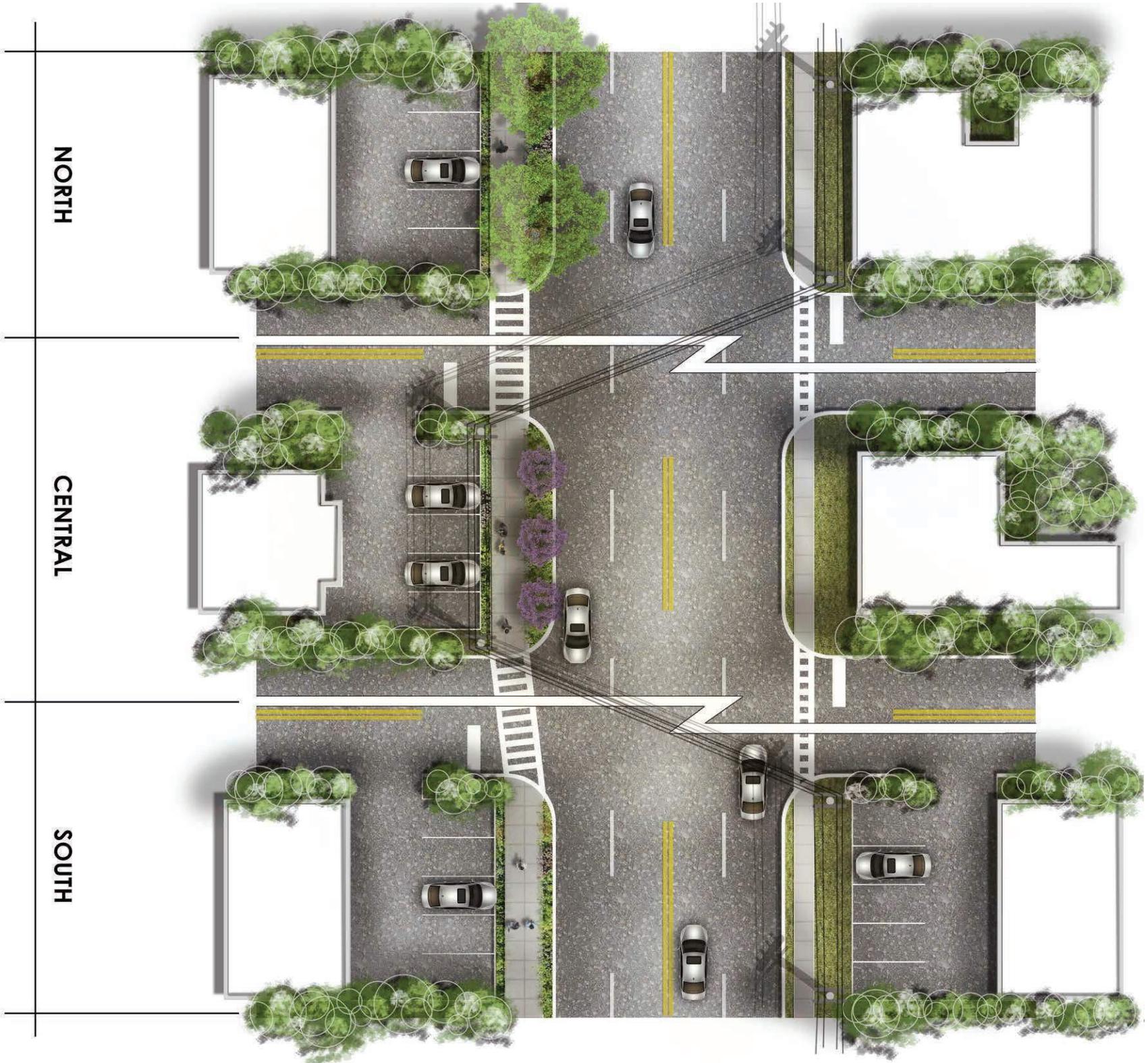




Franklin Road

ROW 75'-80'

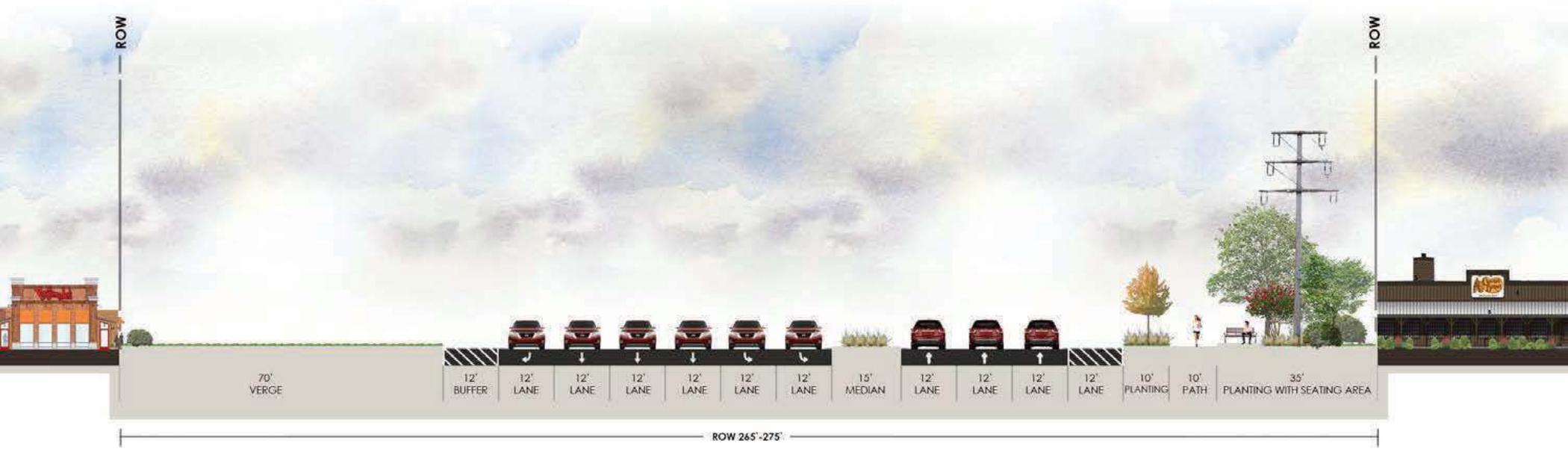
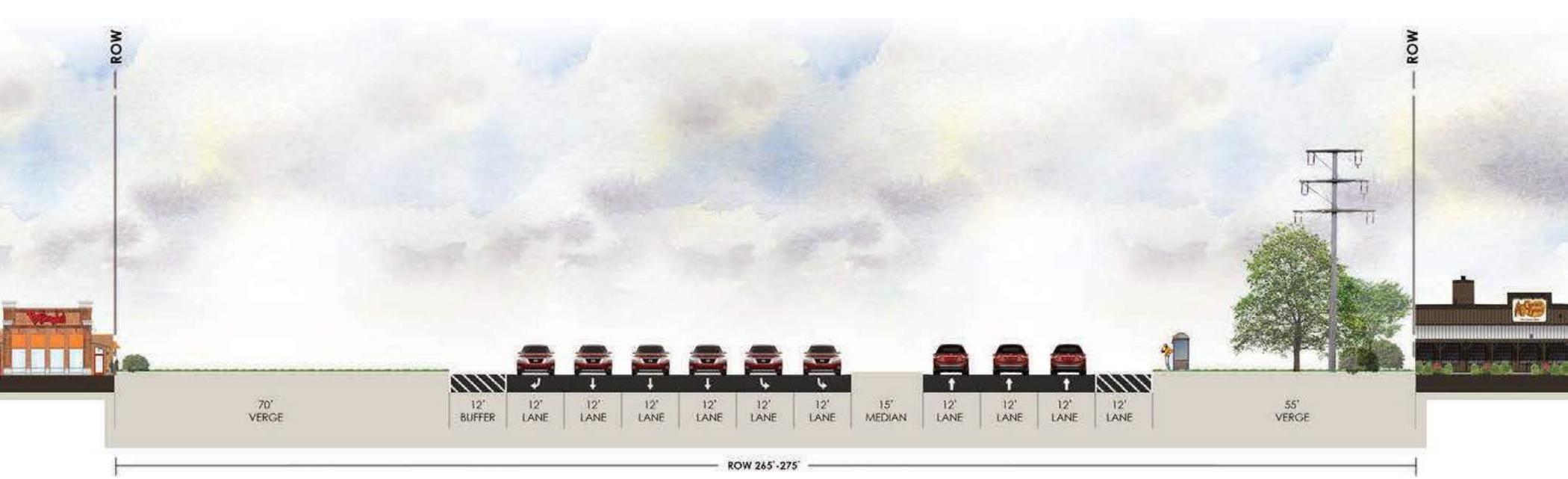




NORTH

CENTRAL

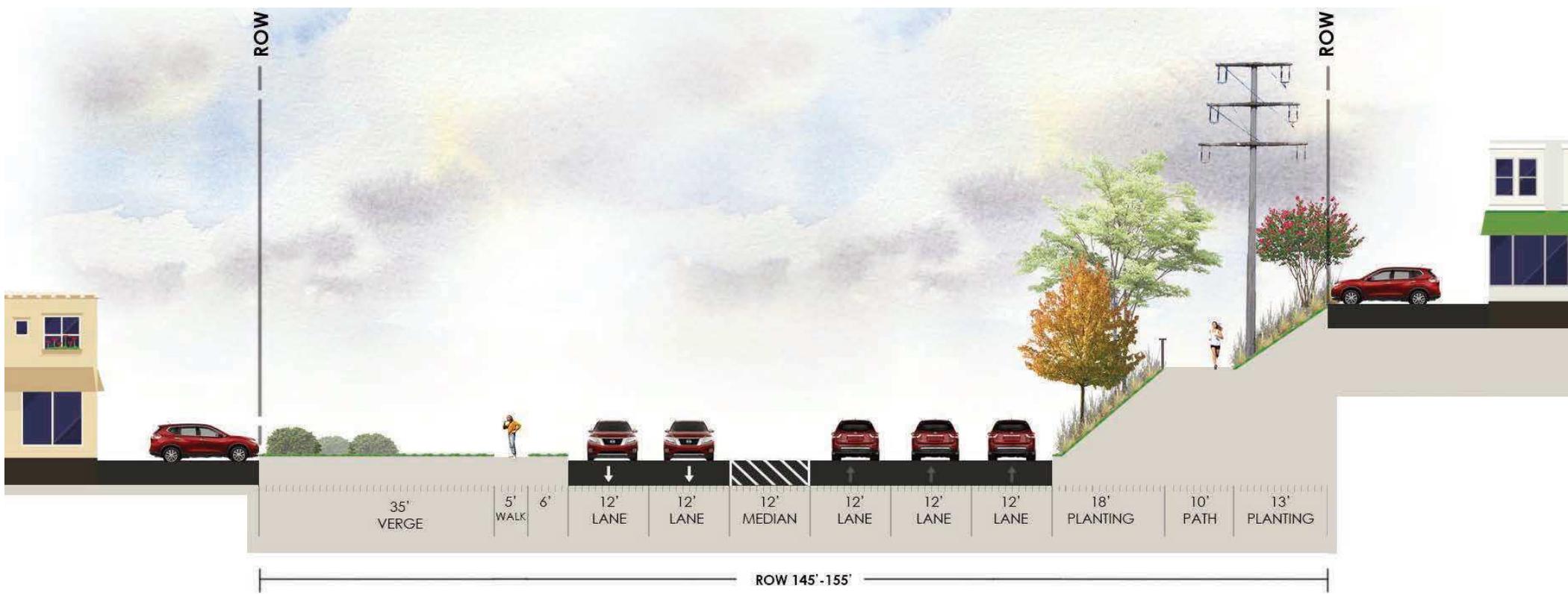
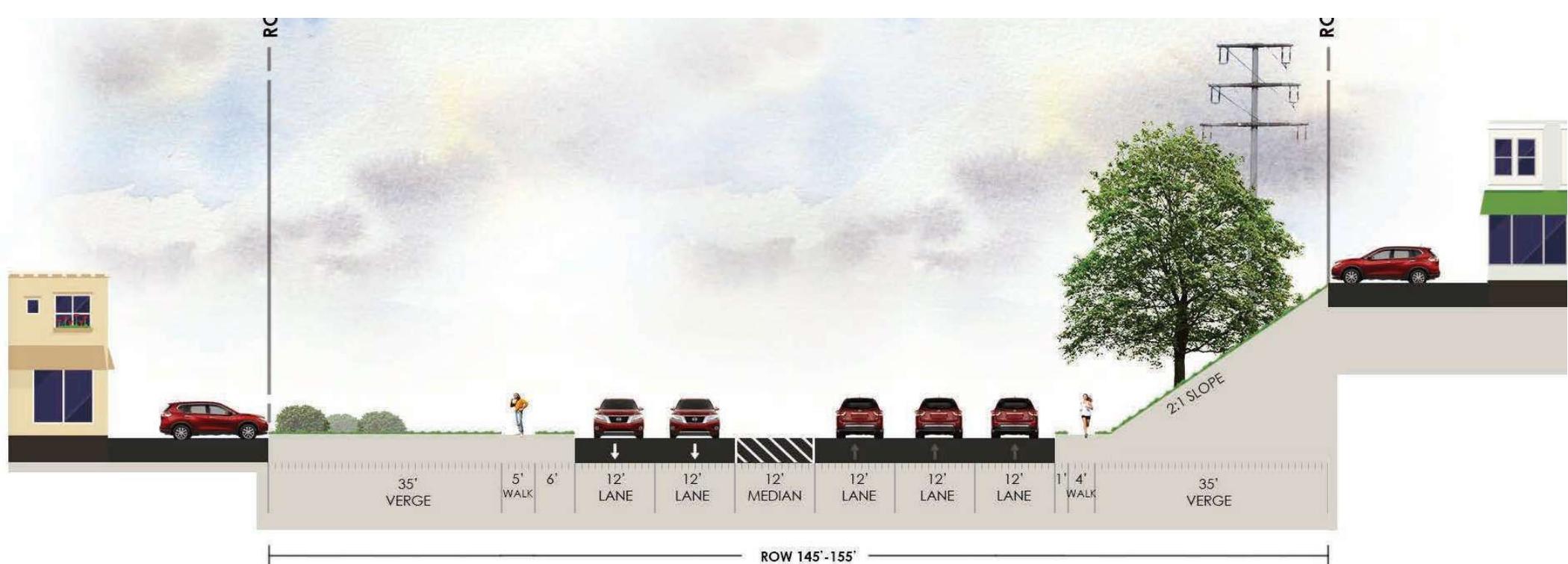
SOUTH

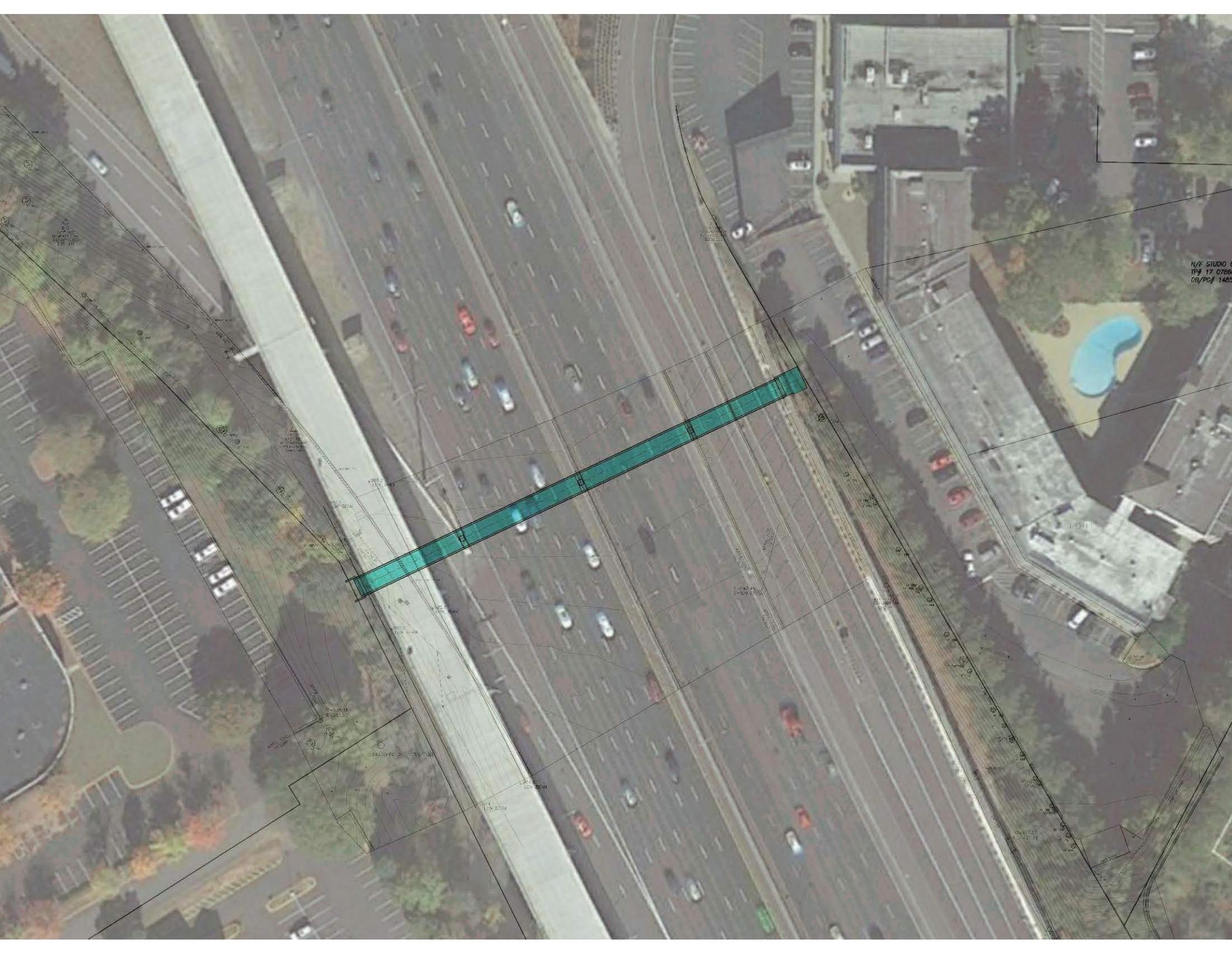


Delk Road





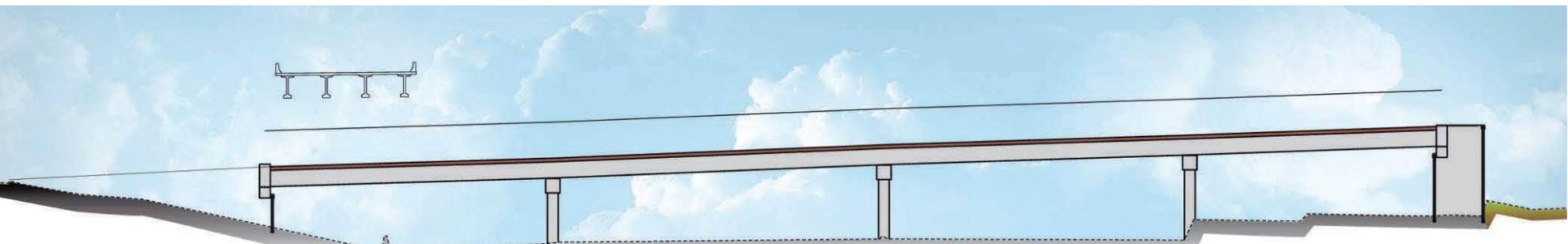
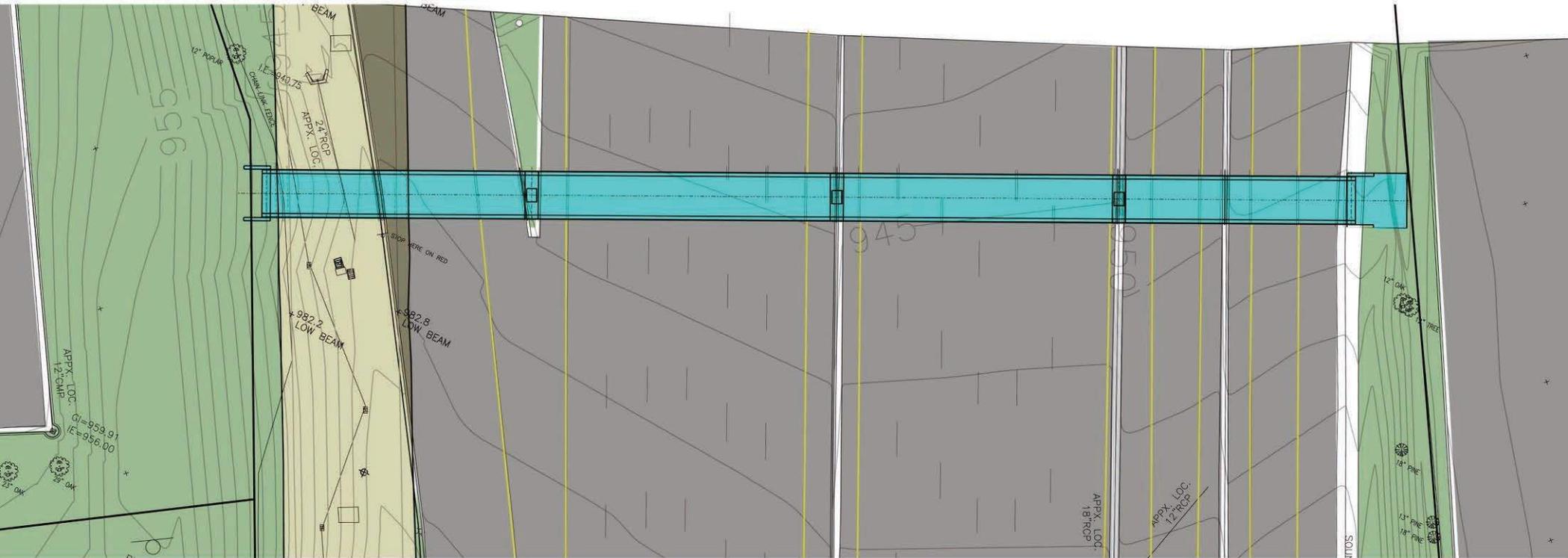


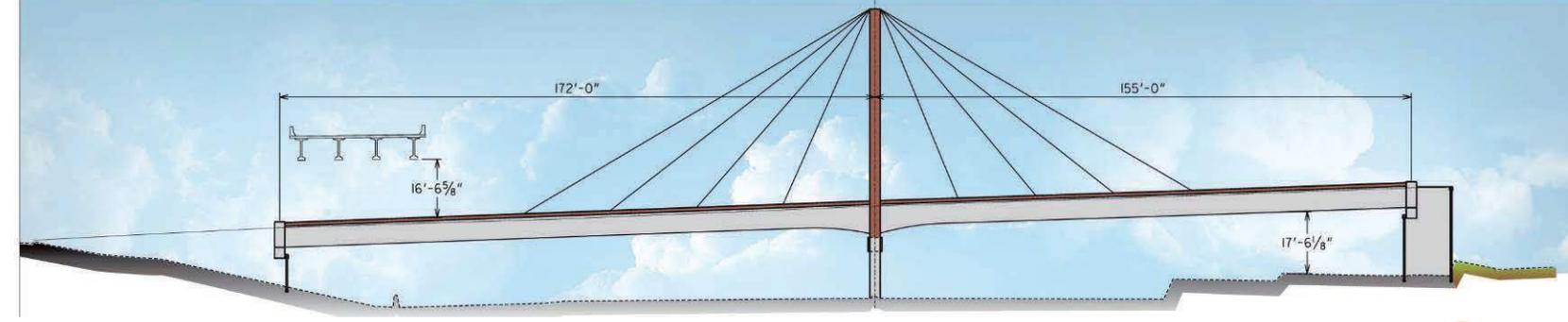
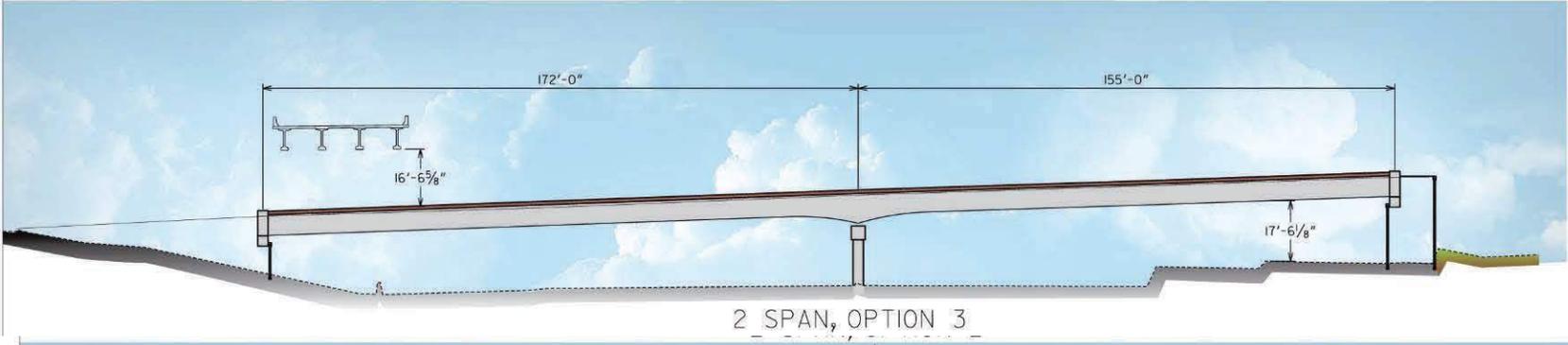
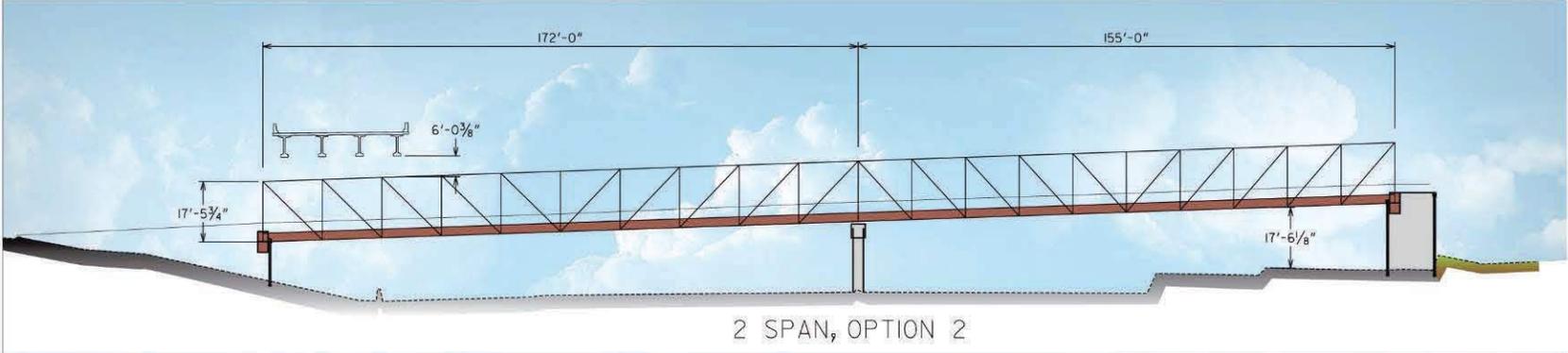
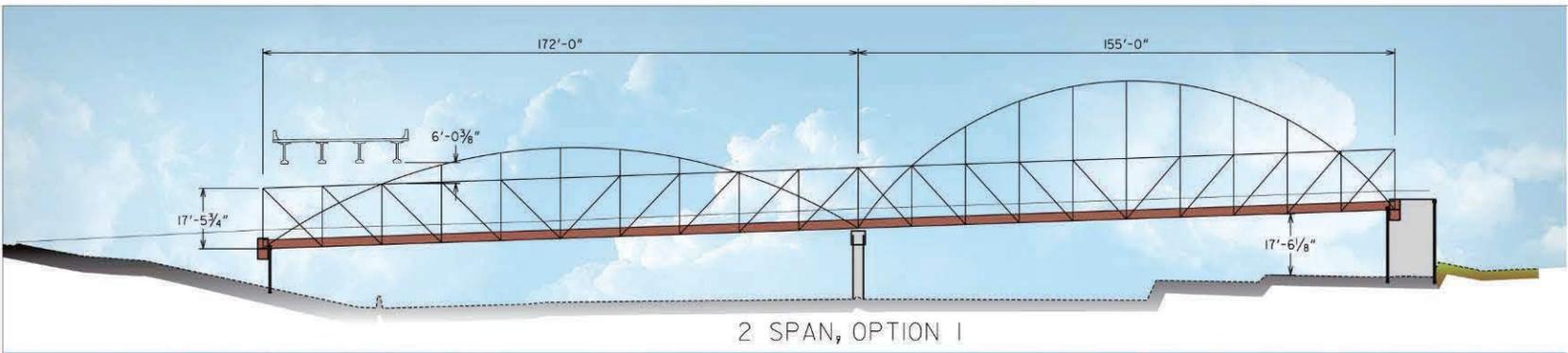


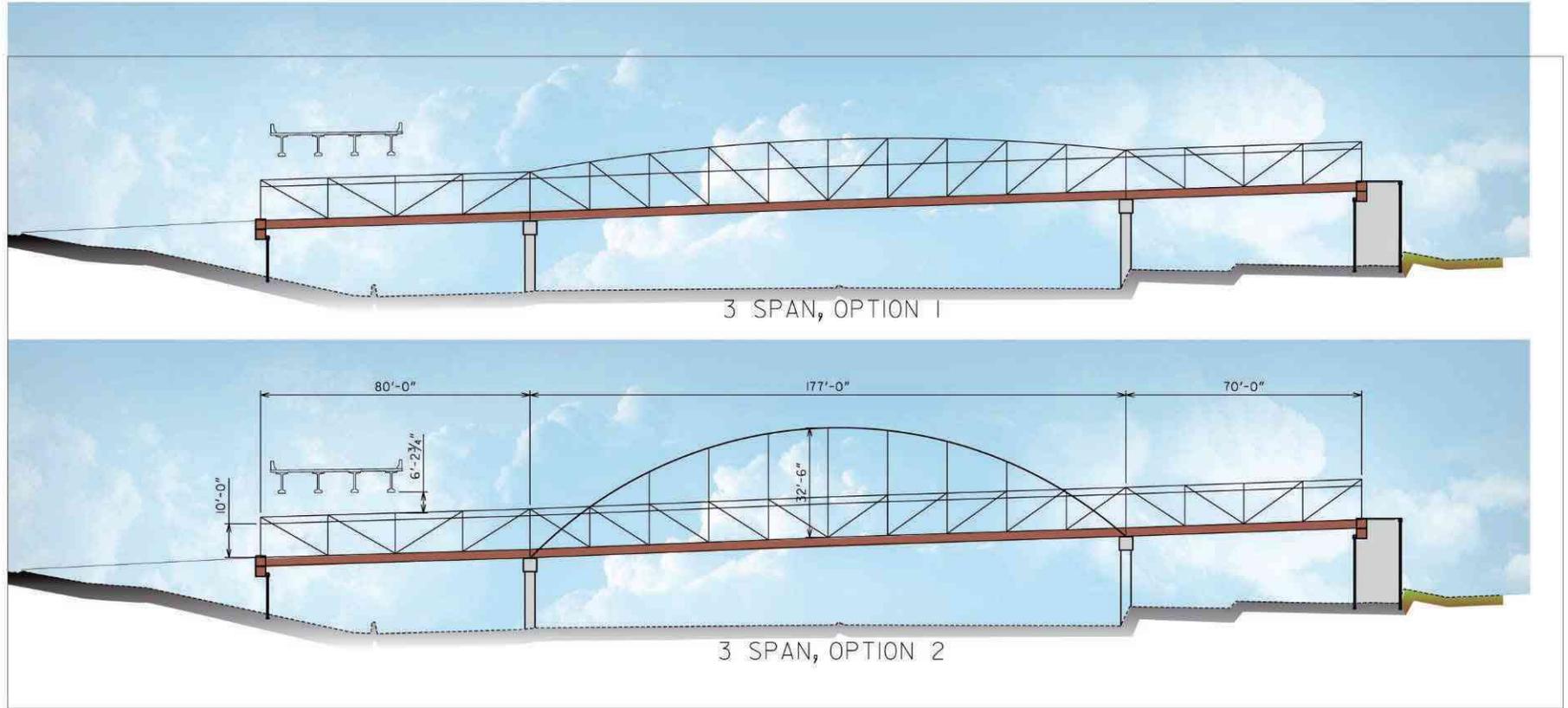
N/F STUDIO
TP# 17 0788
DB/PG# 1455

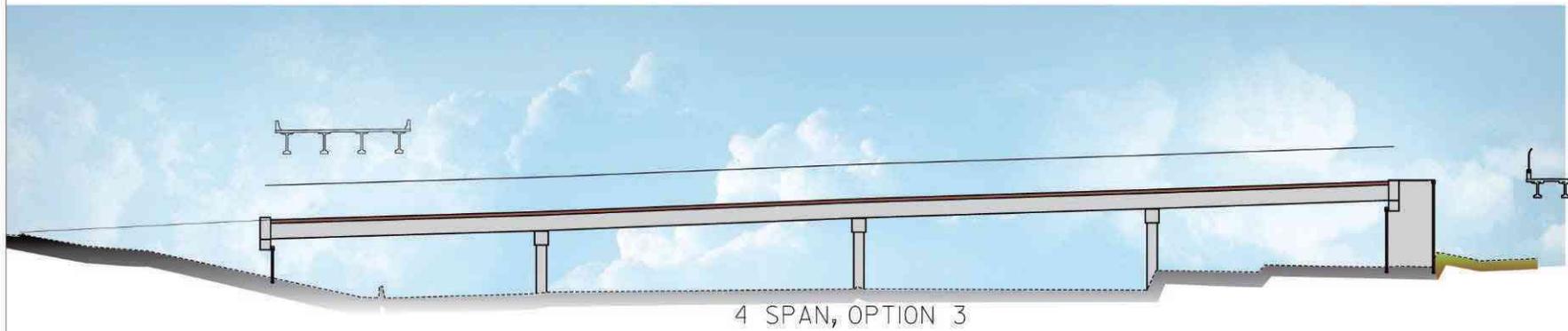
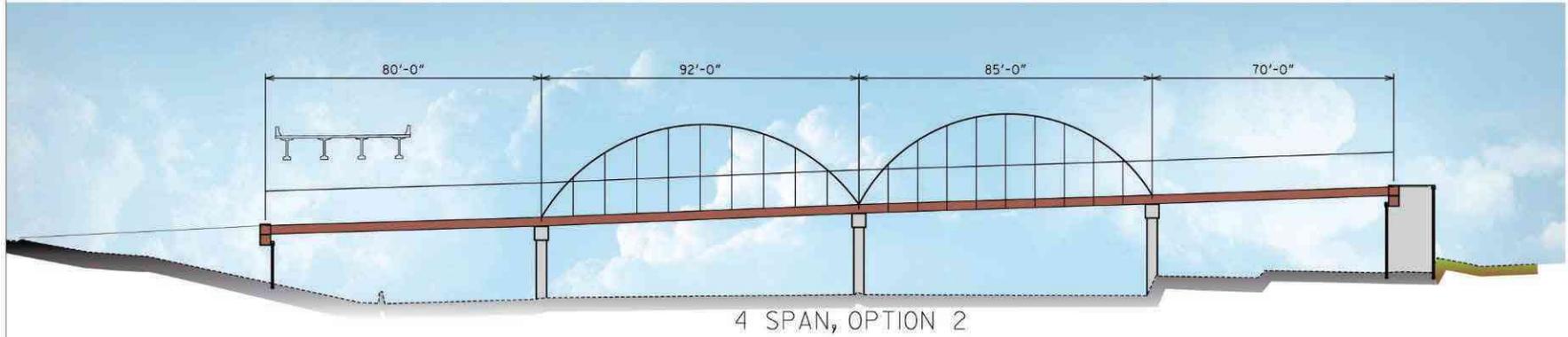
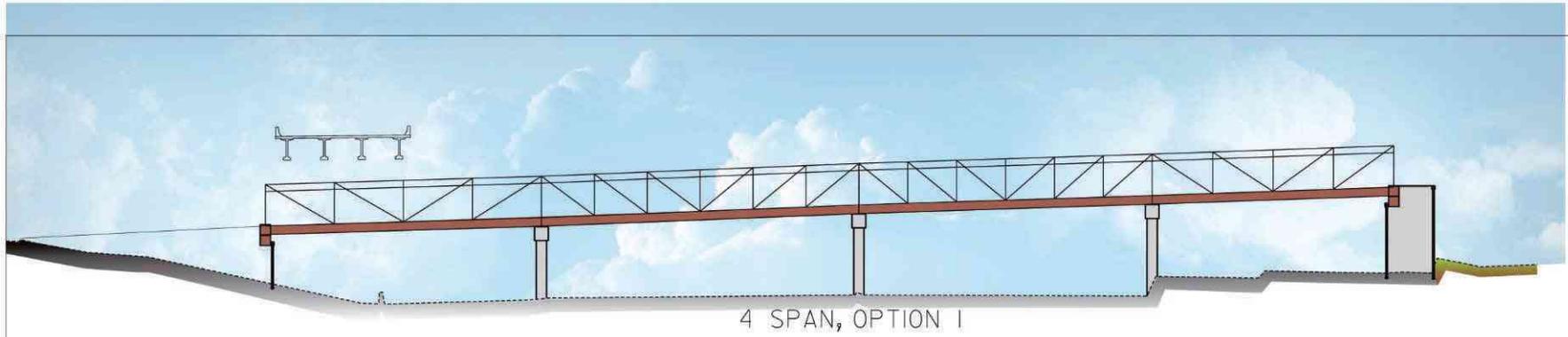












LOOKING FOR THAT INSTAGRAM MOMENT.....

DESTINATION

PLACE



STATUS



EXPERIENCE

Status and Schedule

- PIOH was held Thursday November 21st
- Draft Concept Report has been submitted January 29,2020
- Hold Concept Team Meeting end of February
- Submit Final Concept Report for Approval Mid/Late March
- Respond to any comments from the Concept Report Submittal

**OPD PROJECT MANAGER
CONCEPT TEAM MEETING PRESENTATION & AGENDA
PI Number 0015049, Cobb County
Rottenwood Creek Trail Phase II – Scoping Only**

Date: February 26, 2019

Location: One Georgia Center, GDOT, 4th Floor

- 1. WELCOME – *Olusola Adekonojo***
- 2. INTRODUCTION OF EACH ATTENDEE – *Olusola Adekonojo***
- 3. PROJECT IDENTIFICATION – *Olusola Adekonojo***
- 4. POWERPOINT PRESENTATION – *Stantec (attached)***
- 5. PLANNING**
 - **Traffic Projections**
 - Existing traffic data will be presented in the concept report
 - No proposed traffic will be shown as the project does not propose roadway improvements.
- 6. BRIDGE DESIGN**

Consultant structural team has reviewed existing conditions and conceptualized the base structure of the pedestrian bridge.
- 7. DETOURS**
 - No detours are currently planned.
- 8. STATE OR DISTRICT RIGHT OF WAY**
 - There are a couple parcels near the proposed pedestrian bridge that may have a significant impact on the project R/W budget.
- 9. ENVIRONMENTAL**

An environmental screening memo has been submitted for this project.

 - **HISTORY**
 - **ECOLOGY**
 - **AIR/NOISE**
 - **ARCHAEOLOGY**

- **COMMUNITY RESOURCES**
- **ENVIRONMENTAL JUSTICE**
- **PUBLIC INVOLVEMENT**
 - PIOH was held in November 2019
- **PERMITS**
- **UNDERGROUND STORAGE TANKS**

The Phase I study did find 3 gas stations within the project boundary that the project could potentially impact.
- **HAZARDOUS WASTES**

No hazardous wastes were identified during environmental research.

10. UTILITIES

- SRTA is a present utility at the Northwest Corridor
- ELECTRICAL-City of Marietta
- TELEPHONE/COMMUNICATIONS
- WATER/SEWER – City of Marietta
- CABLEOTHERS

11. LOCAL GOVERNMENT AGREEMENTS

- Mowing and Maintenance Agreements will be needed
- Lighting Agreement will be needed.

12. OFFICE OF DESIGN POLICY COMMENTS

13. OFFICE OF ENGINEERING SERVICES COMMENTS

14. OFFICE OF INTERMODAL COMMENTS

15. STATE OR DISTRICT MAINTENANCE COMMENTS

There are no known maintenance issues on the project.

16. OFFICE OF MATERIALS AND TESTING COMMENTS

17. OFFICE OF PROGRAM DELIVERY COMMENTS

- The City will pay for specialty or proprietary items not allowed as part of funding source.
- The project construction is not currently funded. Further consideration will be given to budget limitations when project funding is received.

18. OFFICE OF ROADWAY DESIGN COMMENTS

- The preferred alternate needs to show length of project and termini.

19. STATE OR DISTRICT TRAFFIC OPERATIONS COMMENTS

- Will ICE be required on this project?

20. CONCEPT COMMENTS WITH CONSULTANT RESPONSES

General:

SR 280 (Delk Road (SR 280))?? Multiple instances throughout report.

Delk Road is SR 280 west of I-75 and is not a state route east of I-75.

What design standards/guidelines are being utilized for the Shared-use path? Guide for the Development of Bicycle Facilities? PROWAG

The project will utilize GDOT pedestrian standards and PROWAG. Certain standards will be incorporated and utilized based upon the condition and location of the pathway. This considers whether the pathway is near a roadway or away from the roadway such as out in the woods near Rottenwood Creek. One of biggest differences will be that the gradient slope for the trail shall be no greater than 5% away from the roadway utilizing PROWAG standards.

Recommend reviewing document for wording that states the project is *...increasing safety, making safer, safe, etc.* and reword or remove.

Different language will be considered to describe the intent of the project that describes the purpose of the proposed enhancements and additions.

All pages should either be sized at 8.5 x 11 or 11 x 17. All text should be readable when printed.

All pages will be these sizes. Stantec will go back and review text to be readable on print outs.

Cover: Recommend rewording/reducing description and keep the State Transportation Planning Administration signature line together with other signatures.

Cover page will be reworked to ensure all signature lines fall on one page as suggested

Location Map:

Please update to a roadway network map in place of an aerial image.

A roadway network map will be added in place of the aerial image.

Project Justification Statement: Please review concept report 'blue guidance'.

Has the Planning Office reviewed the PJS and approved?

No, justification statement will be provided to the planning office for review.

Multiple instances of wording that states the project is *...increasing safety, making safer, providing safer, create safe, etc.* Can not assert this since individual actions are involved.

Different language will be considered to describe the intent of the project that describes the purpose of the proposed enhancements and additions.

Paragraph 1, 1st sentence - Please avoid stating the solution. Actually entire paragraph could be deleted. Paragraph 3, 1st sentence – delete sentence. Review remaining statement to address. Many items should be covered under the Description of Proposed Project on page 5.

CONCEPT TEAM MEETING AGENDA
PI Number 0015049, Cobb County
Rottenwood Creek Trail Phase II -Scoping Only
February 26, 2019
Page 4 of 8

PJS will be reviewed to be paired down as suggested. Additional review of content will be made to not duplicate what should instead be covered under Description of Proposed Project.

Existing Conditions: Recommend adding Interstate 75 (since proposed pedestrian bridge spanning)
I-75 description will be added in the existing conditions section.

Projected Traffic:

Is there projected traffic for the shared-use path? Bicycles and pedestrians.. Trucks % would be N/A
There is not projected traffic for the pathway for bicycles or pedestrians. The traffic volumes were presented for Delk Road as being the main corridor that the pathway corresponds to. This will be designated next to the traffic numbers. The truck traffic is also shown for Delk Road as well.

Projected Traffic shown: please identify roadway volume and % is for – assume is Delk Road
Projected traffic is for Delk Road. All roads and callouts will be made clear and identified further for clarity.

AASHTO Functional Classification: (Mainline). Add (Shared-Use Path): N/A

- The AASHTO items listed appear to be for Delk Road
- AASHTO Functional Classification: (Mainline) – Recommend replacing parenthesis to (Delk Road) as that seems to be roadway referred to.
- AASHTO Context Classification: (Mainline) – Recommend replacing parenthesis to (Delk Road) as that seems to be roadway referred to.
- Parenthesis delk
- AASHTO Project Type: (Mainline) – Recommend replacing parenthesis to (Delk Road) as that seems to be roadway referred to and Construction on Existing Road

These callouts will be considered and revised as suggested to ensure clarity of what areas are being called out or identified.

NHS route: Please Verify, Delk Road does not appear to be an NHS route per GDOT Functional Classification Mapper. Interstate 75 is however.

The intent was to suggest 75 was a NHS route. Since multiple roads are represented on this project, specific callouts will be identified for the road and features.

Complete Street Warrants: List each Warrant Standard number and Guideline number met briefly underneath section. (*saw warrants discussion in the attachments*)

Exact warrant numbers and guidelines will be listed.

Pavement Evaluation: remove note.. note needed. If crosswalk modifications are anticipated, those should be identified elsewhere and costs included in CES.

Note will be removed as no pavement evaluation will be needed.

RTOP: Intersects an RTOP

Will be revised.

FAA: please verify (email link is in concept report template)

This will be verified.

Design Features Table –

Has the design team coordinated with Office of Design Policy regarding Design Variances (*early coordination is required if conceptual design as presented depends on DV/DE being allowed*)

Stantec will coordinate design variances with ODP prior to final concept report submittal.

CONCEPT TEAM MEETING AGENDA
PI Number 0015049, Cobb County
Rottenwood Creek Trail Phase II -Scoping Only
February 26, 2019
Page 5 of 8

Add a Design Features Table for Shared Use Path showing the minimum - maximum information as provided from the appropriate Guide/Standards allowed (e.g. design speeds, vertical grade, horizontal alignments, etc.). Add reference where information for 'Policy' column is obtained. List Rows for paralleling existing roadway and for New Location as maximum allowable grades are different. New Location section: 5% - maximum vertical grade per PROWAG must be met (Greater than 5% is only allowed where proposed shared-use path parallels road in normal shoulder area).

A design features table will be added for the shared use path. The guidelines and standards utilized for the pathway whether it be on a roadway or off a roadway will be identified in all tables and designations.

Lateral Obstruction - -coordinate now for what will be allowed
Lateral obstruction will be coordinated now with ODP.

Design Variances: *please see example comments (➤) issued for a similar shared-use path project regarding variances below*

Intersection Sight Distance – please check undetermined and add note below.
Sight distance will be checked as undetermined.

Lateral Offset to Obstruction – Add trees to your note after ‘...landscape,’

- *Lateral Offset - A design variance will not be permitted to place trees and shrubs less than the minimum lateral offsets to trees and shrubs as defined for the speed limit in the Design Policy Manual. This topic has been discussed in depth with Districts, Traffic Operations and upper management. Limiting visibility is not acceptable to the Department.*

‘trees’ will be added after landscape callout and our team will re-evaluate tree placement as needed to not impede on the minimum lateral offset.

Intersection Sight Distance - New features causing intersection sight distance to not meet will likely not be approved.

There are not any proposed features that are anticipated to affect sight distance.

Verify if median usage is required since is an existing median and no change to width other than creating the refuge?

Median will be evaluated and exact impacts to existing median will be clarified in the concept report.

Lighting: Note Lighting Commitment Letter included in attachments

Will be noted that the commitment letter can be found in the attachments.

TMP: Verify with Planning regarding TMP (whether significant or non-significant) since proposing ped bridge over Interstate 75.

This will be verified. Based upon previous GDOT bridge conversations regarding spans and support placement, it is our understanding the TMP will be significant on I-75 while it should remain as non-significant for the rest of the pathway corridor.

Major Intersections: Add Interstate 75

We can add I-75, but since the project does not actually intersect I-75 with a direct connection. The project intersects the I-75 footprint, but the final user will not intersect I-75. Does this matter?

ICE: Verify with Office of Traffic Operations if an ICE report is needed for this shared-use path

We will verify if the ICE waivers are required. If they are, the approved waivers will be included in the final concept report.

CONCEPT TEAM MEETING AGENDA
PI Number 0015049, Cobb County
Rottenwood Creek Trail Phase II -Scoping Only
February 26, 2019
Page 6 of 8

SUE: Most likely will be Yes – Verify with District 7 for what they will require and update Concept Utility Report.

This will be changed to yes as we have now received multiple requests from SRTA to consider their underground utility for the Northwest Corridor.

Impacts to USACE property: Is the shared-use path in a floodplain?

Portions of the pathway will be in a floodplain. Exact impacts, if any, to the 25' stream buffer are yet to be determined and will be considered during the engineering phase.

Major Stakeholders: Add City of Marietta, SRTA

These two groups will be added to the stakeholders list.

Air Quality: Ozone checked Yes. Add statement below per concept report 'blue guidance'

Note will be added per the guidance.

Project Cost Table:

Revisions to Programmed Costs indicates \$2,000,000 for ROW? Add Notes under table indicating that Cost Estimates were developed by design team.

Design team note will be added for the R/W estimate.

Estimated Cost Total: Should be the sum of all columns.

The estimated costs are included as what the project may actually cost. The programmed costs show what has been allocated to the project. Since construction and other items are not yet programmed it seems that we should demonstrate what the actual estimated costs are and then show the difference between the estimated costs and what has been programmed to this point.

Attachments:

Mitigation Estimate is missing (include after ROW Estimate)

Based upon environmental study, no environmental mitigation is anticipated at this time.

Add signed Lighting Commitment Letter

Letter will be attached to concept report.

Remove Environmental Screening Memo

Memo will be removed from the Concept Report

Concept Layout:

No Construction easements are shown and proposed path is at edge of existing ROW in places.

Anticipated construction easements will be shown on the concept layout.

Delk Road intersection with Franklin Gateway: How will the crosswalks and islands be updated to accommodate bicycles and pedestrians?

Concept for improvements at this location including crosswalk and median will be included in the concept report.

Pedestrian Bridge over I-75: Shared use path Entrance and Exit turns are 90 degrees and potential conflict points for pedestrians and cyclists. How will a bicycle negotiate the turn without encroaching into path of others?

The current concept layout is a 'concept' for the bridge. The exact pathway layout may vary as there are considerations for plaza space or other types of ramp features that will affect the turning radius before and after the pedestrian bridge.

CONCEPT TEAM MEETING AGENDA
PI Number 0015049, Cobb County
Rottenwood Creek Trail Phase II -Scoping Only
February 26, 2019
Page 7 of 8

Typical Sections:

PLEASE Reduce # of Typical if possible. Existing features are normally included in proposed typical section.

Number of typicals will be evaluated and reduced as possible by utilizing a ‘varies’ with a range of dimensions.

Add Details showing minimum distance to lateral obstruction (tree), and minimum distance to lateral obstruction (lighting standard)

Details will be added to demonstrated the lateral obstructions for both lights and trees.

Add Roadway Names to Typical Sections

Road names will be added to all typical sections. Typical sections do show the station locations.

TS AA Right CL border area existing is 23.73 feet dimensioned, Proposed is 24.73’ dimensioned ?

Dimension discrepancy will be verified and revised as necessary.

TS BB & CC: what is different between Existing and Proposed?

TS BB shows an existing Pedestrian crossing over Rottenwood Creek. This typical will be removed and captured within a realm of ‘varies’ and dimension ranges.

TS DD – Update Font size to readable when printed and similar to prior typicals.

Font size will be ‘upsized’

ES (page 43): Missing Crosswalk items, Pedestrian Signal upgrades, etc.

Missing items for crossings will be added.

Revisions to Programmed Costs Worksheets: Old forms (*Current form spreadsheet is dated 9/18/19, Erik Rohde; Go to ROADS, Design Related Resources, Engineering Services*) Note costs are by design team if applicable.

Will review and update as needed.

ROW Estimate Summary: missing if developed by design team add a * Note indicating

Note will be added that estimate was created by design team.

Utilities Cost Estimate: *Please obtain Utilities cost Estimate form from District 7 and add Note: Utility Cost Estimate Developed by Design Team and incorporate costs from pages 47 – 54 into it and remove these pages.*

Form will be obtained and updated with note also added regarding design team.

Concept Utility Report: Note developed by design team? Revise SUE to Yes and add note beside Level * Undetermined

Note will be added and SUE will be updated to a YES checkbox.

ICE Waivers – include approved waivers in CR

We will verify if the ICE waivers are required. If they are, the approved waivers will be included in the final concept report.

MS4 – Review Concept Report blue guidance for appropriate MS4 items to include.

Remove unnecessary information such as (*Pages 83 & 84, Pages 92-96, 2.4 Appendix D pages 103-106, 2.5 Appendix E pages 107 -146, 2.6 Appendix F Soil Resource Report pages 147-184*)

The MS4 report will be revised as suggested to reduce the overall size of the MS4 section.

21. ADDITIONAL COMMENTS & CONCERNS FROM ATTENDEES

- ❖ Comments given before or during this meeting will be responded to in the published meeting minutes.
- ❖ Disclaimer – Please refer to the approved concept report for the final, documented design decisions.

Attachment 10

Minutes of any meetings that shows support or objection to the concept

- a. PIOH November 21, 2019
- b. Public Outreach during Marietta concert series
 - a. September 19, 2019 mid-day
 - b. September 27, 2019 evening
- c. GDOT meetings with specific discipline teams
 - a. 8-3-2017 GDOT Environmental, OPD, Environmental, Bridge
 - b. 8-16-2017 GDOT roadway and traffic
 - c. 8-21-2017 GDOT Structural



Department of Development Services
205 Lawrence Street
Marietta, Georgia 30060
Rusty Roth, AICP, Director

December 12, 2019

Re: Responses to Open House Comments for PI#: 0015049, Cobb County, Rottenwood Creek Trail – Phase II

Thank you for your comments concerning the proposed project referenced above. We appreciate your participation and all of the input that was received as a result of the November 21, 2019 Public Information Open House (PIOH). Every written comment received and verbal comment given to the court reporter will be made part of the project's official record.

A total of **7** people attended the open house. Of the **2** respondents who formally commented, **2** were in **support** of the project, **0** were **opposed**, **0** were **uncommitted**, and **0** expressed **conditional support**.

The attendees of the open house and those persons sending in comments within the extended comment period raised the following questions. The City of Marietta has prepared this one response letter that addresses all comments received so that everyone can be aware of the questions raised and the responses given. Please find the comments summarized below (in *italics*) followed by our response. In instances where numerous questions or comments were received regarding similar subject matter, those items were combined to aid the reader in their review of the expressed concerns.

1. *I think this will be a wonderful asset to the area. / I like the idea of having more trails in the area.*

Response: Thank you for your comment.

2. *The biggest issue I have with Marietta and Cobb County trails is the lack of restrooms.*

Response: Thank you for your comment. In regards to the Rottenwood Creek Trail specifically, the City will be considering design features that include restroom facilities at trailheads as we move further into the design phase.

Again, thank you for your comments. Should you have further questions, comments or concerns, please call the project manager, Courtney Verdier, at 770-794-5717.

Sincerely,

A handwritten signature in blue ink that reads "Courtney Verdier".

Planning & Economic Development Specialist

CV/TJ
PDF for Project File

GDOT Meeting (0015049) - CobbRottenwood Phase II / 178460151

Date/Time: August 3, 2017 / 10:00 AM
Place: GDOT Office
Next Meeting: Next Meeting Date
Attendees: Oluchukwu Anyaebosi, GDOT-OPD; Rachel Perrine, GDOT Archaeology; Chad Carlson, GDOT Hist.; Stephen Hopper, Stantec; Miles Kemp, GDOT OES; Andrew Kohr, Stantec; Nathan Aarons, Stantec; Alania Stewart, GDOT Area 2; Dave Peters, GDOT Design Policy; Steve Gaston, GDOT Bridge; Diana Fields, ARC; Amber Barlow, GDOT NEPA; Dan Conn, Marietta; Courtney Verdier, Marietta; Daniel Cummings, Marietta; Sam Samu, GDOT-OPD
Absentees: Absentees
Distribution: Attendees; Marietta PMT

Project Overview

Andrew Kohr and Stephen Hopper gave a presentation on the background of the project and the two alternatives that have been proposed at this point.

Each alternative proposes a different method with crossing Interstate 75: 1) The first is crossing at the creek just north of Delk Road using part of the existing culvert or building an additional structure adjacent to the current culvert. 2) The second utilizes the current overpass along Delk Road or fabricating a separate pedestrian bridge. While there are obvious conflicts regarding the crossing of Interstate 75 with both alternatives, additional studies need to be undertaken to understand the complete effects the proposed changes would have on the creek and culvert in the first option or on traffic and the bridge in the second option.

Preliminary Feedback from GDOT

Although representatives from GDOT Traffic, Ecology, and Structure were not at the meeting, some preliminary feedback from those who attended included:

- Go ahead and talk with FHWA
- Identify additional studies necessary to provide a more detailed understanding of the impacts of each crossing (traffic analysis, hydraulic study, etc.)
- There are only three conflict points along the south side of Delk at I-75 and could provide for an easier crossing
- Provide a 5' buffer along Delk or a raised barrier between the trail and the roadway

August 3, 2017

GDOT Meeting
Page 2 of 2

Next Steps

Next steps for the Project Team are as follows:

1. Stantec to meet individually with the GDOT representatives for Ecology, Traffic, and Bridge Engineering to get a better understanding of what needs to be done regarding the two crossing conflicts
2. Schedule a meeting with FHWA to discuss using the culvert
3. Stantec and Marietta will then discuss the additional studies necessary for determining the appropriate trail alignment

The meeting adjourned at 11:30 AM

The foregoing is considered to be a true and accurate record of all items discussed. If any discrepancies or inconsistencies are noted, please contact the writer immediately.

Nathan Aarons, PLA, ASLA, LEED GA
Landscape Architect
Phone: (770) 492-2672
Nathan.Aarons@stantec.com

MEETING SIGN-IN SHEET

Project: 0015049, ROTTENWOOD CRK TRAIL - PH II

Date: August 3, 2017

Time: 10:00 AM

Facilitator: Oluchukwu Anyaebosi

Place/Room: OGC Atlanta, 25CR3L2

Name	Company	Phone	E-Mail
Oluchukwu Anyaebosi	GDOT-OPD	404-631-1540	oanyaebosi@dot.ga.gov
Rachel Perrine	GDOT-Archaeology	404-631-1072	rperrine@dot.ga.gov
Chad Carlson	GDOT Hist.	4-631-1400	ccarlson@dot.ga.gov
STEPHEN HOPPER	STANTEC	7.315.8155	stephen.hopper@stantec.com
Mrles Kemp	GDOT OES	(404) 631-1127	mkemp@dot.ga.gov
ANDREW KOHR	STANTEC	4556.8758	ANDREW.KOHR@STANTEC.COM
NATHAN AARONS	STANTEC	404.403.9066	nathan.aarons@stantec.com
Alania Stewart	GDOT-Arc2	404-326-5545	alstewart@dot.ga.gov
Dave Peters	GDOT/Des. Policy	4/631-1738	dpeters@dot.ga.gov
Steve Gaston	GDOT - Bridge	404-631-1881	sgaston@dot.ga.gov
DIANA FIELDS	ARC	470-378-1646	dfields@atlantaregional.com
Amber Barlow	GDOT-NEPA		
DAN CONN	MARIETTA	7707945658	DCONN@MARIETTA GA. GOV
Courtney Verdier	Marietta	(770)794-5717	cverdier@mariettaga.gov
Daniel Cummings	Marietta	(770)794-5668	dcummings@marietta-sc.gov
Sam Samu	GDOT-OPD	404.631.1545	ssamu@dot.ga.gov

GDOT Concept Team Meeting

Rottenwood Creek Phase II / 178460151

Date/Time: August 16, 2017 / 3:00 PM
Place: GDOT District 7 Office
Next Meeting: TBD
Attendees: Nathan Aarons, Stantec; Stephen Hopper, Stantec; Oluchukwu Anyaebosi, GDOT-OPD; Chris Woods, GDOT-Traffic; Peter B. Emmanuel, GDOT-OPD
Distribution: Project Management Team

Item:

Introductions and Project Background

Stephen and Nathan gave a brief description of the project background, location, and the two alternatives agreed upon to this point.

Traffic Discussion Regarding Delk Road

- GDOT-Traffic is firmly against using the north side of Delk Road when crossing I-75
 - Interrupting traffic on Delk entering on to I-75 is preferred to interrupting traffic exiting on to Delk from I-75
- Safest crossing with regards to traffic conflicts would be to use the existing culvert or tunnel under I-75
 - Option 1 (Culvert) is preferred / Option 2 and 3 (Delk Road) are probable if we use the south side
- GDOT does not want pedestrians directed towards the ramps of I-75 (no perpendicular crossings?)
 - Windward / 400 Northbound exit – Signalized crosswalk similar to what we need for the ramps of I-75
- Extensive traffic study would be needed to have any chance of narrowing lane widths or implementing a road diet – A lot of Semi-trucks use Delk Road.
 - Best we could get is 11' wide lanes, if they are not close already.
 - Look at lane widths including 5-inch lane striping.
- Any barriers added to bridge would need to be tested to make sure bridge can handle the load.

The meeting adjourned at 4:00 PM



August 16, 2017

GDOT Concept Team Meeting

Page 2 of 2

The foregoing is considered to be a true and accurate record of all items discussed. If any discrepancies or inconsistencies are noted, please contact the writer immediately.

Nathan Aarons, PLA, ASLA, LEED GA
Landscape Architect
Phone: (770) 492-2672
Nathan.Aarons@stantec.com

GDOT Concept Team Meeting

Rottenwood Creek Phase II / 178460151

Date/Time: August 21, 2017 / 10:30 AM
Place: GDOT-Bridge Conference Room
Next Meeting: TBD
Attendees: Nathan Aarons, Stantec; Stephen Hopper, Stantec; Andrew Kohr, Stantec; Oluchukwu Anyaebosi, GDOT-OPD; Clayton Bennett, GDOT-Bridge; Susan Beck, GDOT-Bridge; Peter B. Emmanuel, GDOT-OPD
Distribution: Project Management Team

Item:

Introductions and Project Background

Andrew and Nathan gave a brief description of the project background, location, and the two alternatives agreed upon to this point.

Culvert/Bridge Discussion

- Existing Culvert
 - Built in 1965 and upgraded in 1989
 - Although the wall is collapsing, there is no threat to the structural integrity of the culvert. The lack of a wall could lead to the erosion of the surrounding soil.
 - Debris is and always will be a problem with culverts
 - Based on the FIRM Map, the culvert looks to be under designed
 - 3-5" of settling with 1.5-4' of undermining due to scouring, but it isn't anything to be worried about.
- Design Alternatives
 - Using Existing Culvert
 - Using the existing culvert is possible, but if it is under designed as GDOT believes, then this option would not work as planned
 - Concerns regarding the safety of people due to flooding and "cramped" space.
 - Separate "Tunnel"
 - The most feasible option in regard to cost.

August 21, 2017

GDOT Concept Team Meeting

Page 2 of 2

Item:

- Would need to be designed with the possibility of a flooding event and how the water would drain out of the tunnel
- Would need to be designed for a live load of HL-93 just in case
- Replace culvert with a new bridge
 - Cleanest of the options
 - Would address trail, flooding concerns, ecological impact, and pedestrian safety.
 - Most expensive option and would require multi-agency cooperation
 - High impacts to I-75
- Delk Road
 - Study would need to be done on the load capacity of the bridge if adding "dead loads"
 - There are whispers that a divergent diamond could be in the future of the Delk/I-75 Bridge

The meeting adjourned at 11:30 AM

The foregoing is considered to be a true and accurate record of all items discussed. If any discrepancies or inconsistencies are noted, please contact the writer immediately.

Nathan Aarons, PLA, ASLA, LEED GA
Landscape Architect
Phone: (770) 492-2672
Nathan.Aarons@stantec.com

Attachment 11

Other Items referred to in the body of the report

- a. Complete Streets Warrants
 - i. Pedestrian
 - ii. Bicycle
 - iii. Transit
- b. Potential Bridge Concept Graphics
- c. Phase I ESA Acceptance Memo
- d. Crosswalk potentials at Franklin Gateway and Delk Road
- e. City of Marietta Lighting Commitment Letter
- f. Example Mowing and Maintenance Agreement for City of Marietta
- g. Project Justification Statement Approval

To:	Justin Hatch GDOT	From:	Stephen Hopper Stantec
File:	178460151	Date:	January 27, 2020

Reference: Complete Street Warrants for 0015049 (Cobb)

The Rottenwood Creek Trail – Phase II – Scoping Only Project meets pedestrian, bicycle, and transit warrants based on GDOT’s Complete Street Design Policy. As a component of concept report submittals, this memo outlines how each warrant is met. The following warrants address how this project meets individual standards.

Pedestrian Warrants (Standard)

Pedestrian accommodations shall be considered in all planning studies, and be included in all reconstruction, new construction, and capacity-adding projects which include curb and gutter as part of an urban border area (See Figure 6.3). Pedestrian accommodations shall also be considered along roadways with rural shoulders, which meet any of the following conditions:

1. along corridors with pedestrian travel generators and destinations (i.e. residential neighborhoods, commercial areas, schools, public parks, transit stops and stations, etc.), or areas where such generators and destinations can be expected prior to the design year of the project;

Franklin Gateway Road, Delk Road (SR 280) and Bentley Road contain a mix of commercial, retail, recreational and residential uses that generate (and have the potential to generate) pedestrian traffic. Bordering Franklin Gateway is a popular sports complex as well as a variety of retail and commercial businesses including restaurants, convenience stores and strip shopping centers. Along Delk Road (SR 280) continues a variety of retail and commercial including hotels, restaurants, office parks, and strip shopping centers. Bentley Road is primarily residential, and the close proximity to the commercial and retail along Delk Road (SR 280) generates pedestrian activity. The majority of parcels along Franklin Gateway and Delk Road (SR280) are commercial or retail in nature and have the potential to generate additional pedestrian activity. In addition, Franklin Gateway, Delk Road (SR 280) and Bentley Road are all along Cobb County Transit routes, and there are multiple bus stops each corridor which generate additional pedestrian traffic.

2. where there is evidence of pedestrian traffic (e.g., a worn path along roadside);

There is pedestrian traffic along the corridor including areas that don’t have sidewalks. There is evidence of people walking adjacent to the road on both Delk Road and Franklin Gateway.

3. where pedestrian crashes equal or exceed a rate of ten for a ½-mile segment of roadway, over the most recent five years for which crash data is available;

Current data doesn’t support a high rate of pedestrian crashes.

4. where a need is identified by a local government, MPO or regional commission through an adopted planning study.

The City of Marietta as well as Cobb County has long identified the need to address pedestrian connectivity through a robust and interconnected trail network both on a North-South axis as well as Rottenwood Creek Trails East-West reach. This project is the continuation of that mission along a busy corridor where a need for pedestrian connectivity has been identified. This addresses the pedestrian warrant.

Reference: Complete Street Warrants for 0015049 (Cobb)

Bicycle Warrants (Standard)

Bicycle accommodations shall be considered in all planning studies and shall be included in all reconstruction, new construction, and capacity-adding projects that are located in areas with any of the following conditions:

1. if the project is on a designated (i.e., adopted) U.S., State, regional, or local bicycle route;

City of Marietta and Cobb County have identified this project to be a priority consistent with the bicycle and pedestrian connectivity goals set in ARC's 2030 Regional Transportation Plan (RTP) and the 3-year Transportation Improvement Program (TIP).

2. where there is an existing bikeway along or linking to the end of the project alignment (e.g., shared lane, paved shoulder, bike lane, shared-use path, or cycle track);

There are not any existing bicycle lanes along the project corridor or link to the end of the project alignment.

3. along project alignments with bicycle travel generators and destinations (i.e. residential neighborhoods, commercial centers, schools, colleges, scenic byways, public parks, transit stops/stations, etc.); and

Franklin Gateway Road, Delk Road (SR 280) and Bentley Road contain a mix of commercial, retail, recreational and residential uses that generate (and have the potential to generate) bicycle traffic. Bordering Franklin Gateway is a popular sports complex as well as a variety of retail and commercial businesses including restaurants, convenience stores and strip shopping centers. Along Delk Road (SR 280) continues a variety of retail and commercial including hotels, restaurants, office parks, and strip shopping centers. Bentley Road is primarily residential, and the close proximity to the commercial and retail along Delk Road (SR 280) generates bicycle activity. The majority of parcels along Franklin Gateway and Delk Road (SR 280) are commercial or retail in nature and have the potential to generate additional bicycle activity. In addition, Franklin Gateway, Delk Road (SR 280) and Bentley Road are all along Cobb County Transit routes, and there are multiple bus stops each corridor which generate additional bicycle traffic.

4. where there is an occurrence of reported bicycle crashes which equals or exceeds a rate of five for a 1-mile segment of roadway, over the most recent five years for which crash data is available.

Current data doesn't support a high rate of pedestrian crashes.

Proposed improvements call for creating shared-path along the Franklin Gateway, Delk Road (SR 280), and Bentley Road corridors. This addresses the bicycle warrant.

Transit Warrants (Standard)

Transit accommodations shall be considered in all planning studies and be included in all reconstruction, new construction, and capacity-adding projects that are located in areas with any of the following conditions:

1. transit vehicles: on corridors served by fixed-route transit; and

Cobb County Transit bus service extends along Franklin Gateway Road, Delk Road (SR280) and Bentley Road.

2. pedestrian transit users: within a ¼-mile pedestrian catchment area of an existing fixed route transit facility (i.e., stop, station, or park-and-ride lot). A catchment area is defined by a radial distance from a transit facility per Federal Transit Administration (FTA) guidelines - this includes crossing and intersecting streets.

January 27, 2020

Justin Hatch

Page 3 of 3

Reference: Complete Street Warrants for 0015049 (Cobb)

10 Cobb County Transit Stops exist currently within the project boundary. Within a ¼-mile catchment area of each stop exists an extensive variety and amount of residential, commercial, office, retail and recreational establishments. A significant amount of pedestrian traffic is generated by these sources.

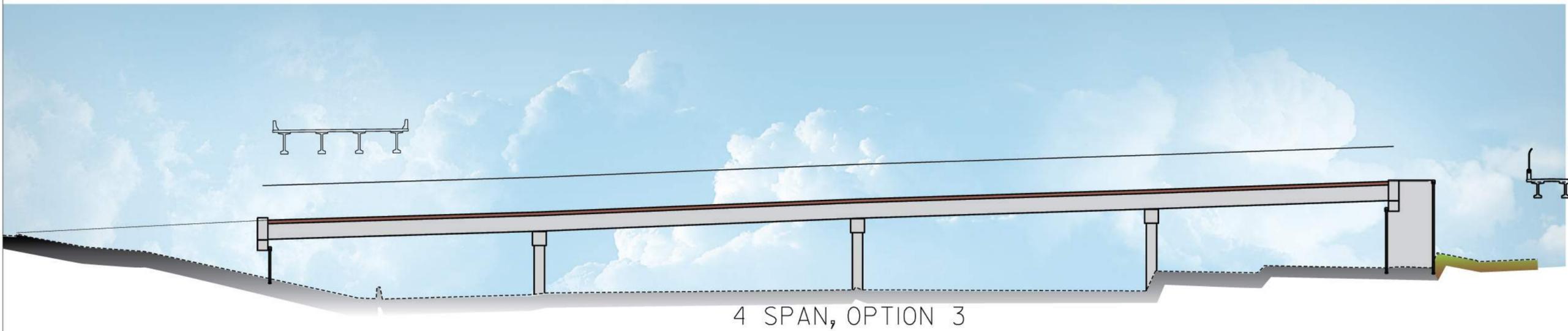
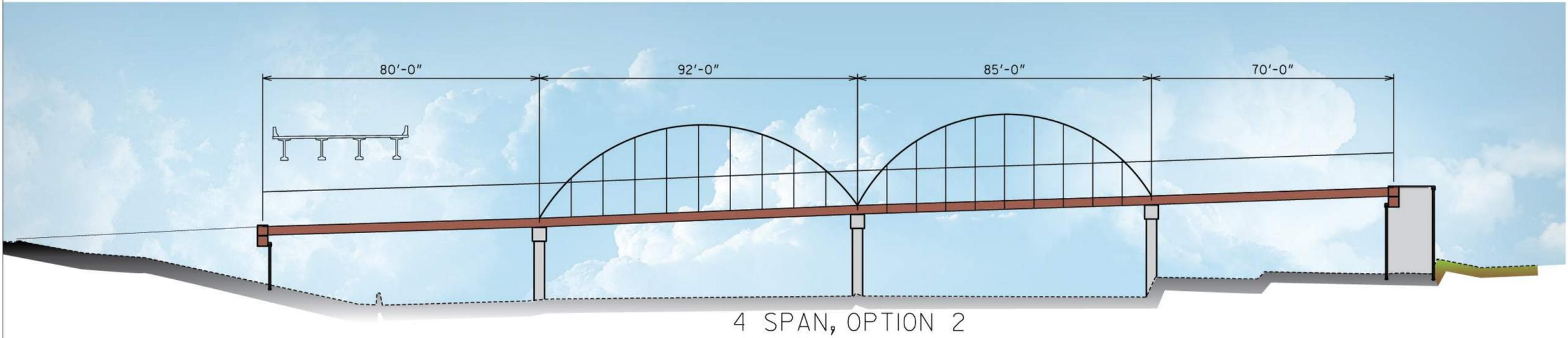
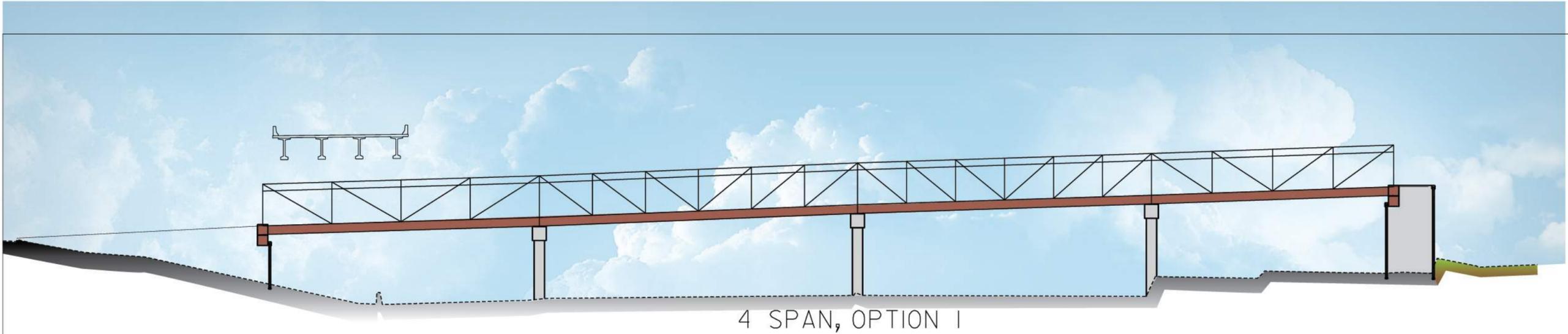
With an enhanced shared-use path, safe access to multiple existing Cobb County Bus Stops is addressed. The Transit Warrant has been met.

Stephen Hopper

Senior Landscape Architect, Associate

Phone: (770) 315-8155

Stephen.hopper@stantec.com



DEPARTMENT OF TRANSPORTATION

STATE OF GEORGIA

INTERDEPARTMENTAL CORRESPONDENCE

FILE Cobb County **OFFICE** Materials and Testing
PI No. 0015049 Forest Park, Georgia
Rottenwood Creek Bike Trail **DATE** November 22, 2019

FROM Monica L. Flournoy, P.E., State Materials Engineer

TO Kimberly Nesbitt, State Program Delivery Administrator
Attn: Olusola T. Adekonojo, Project Manager

SUBJECT **Acceptance of Consultant's Phase I Environmental Site Assessment Report**

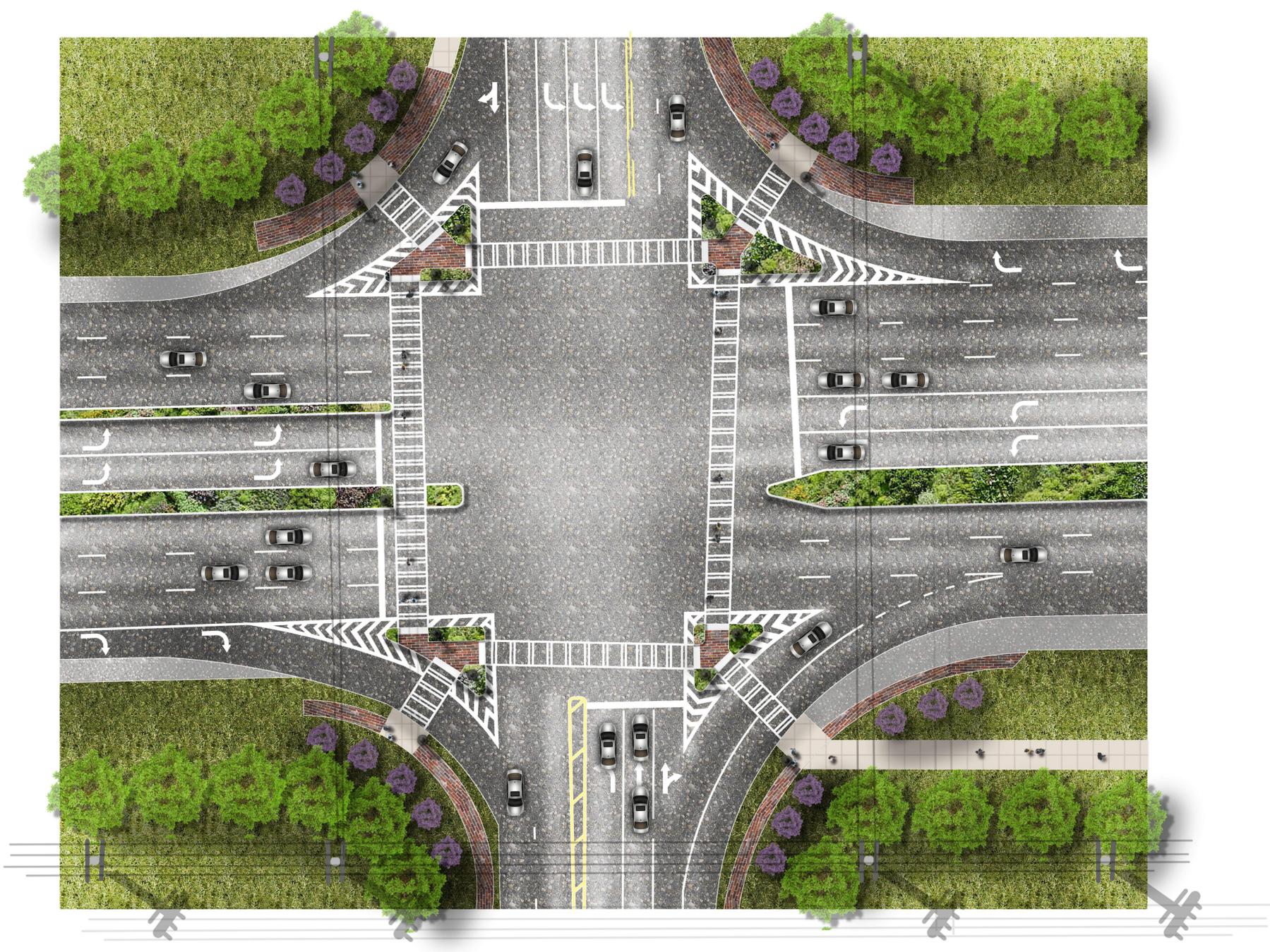
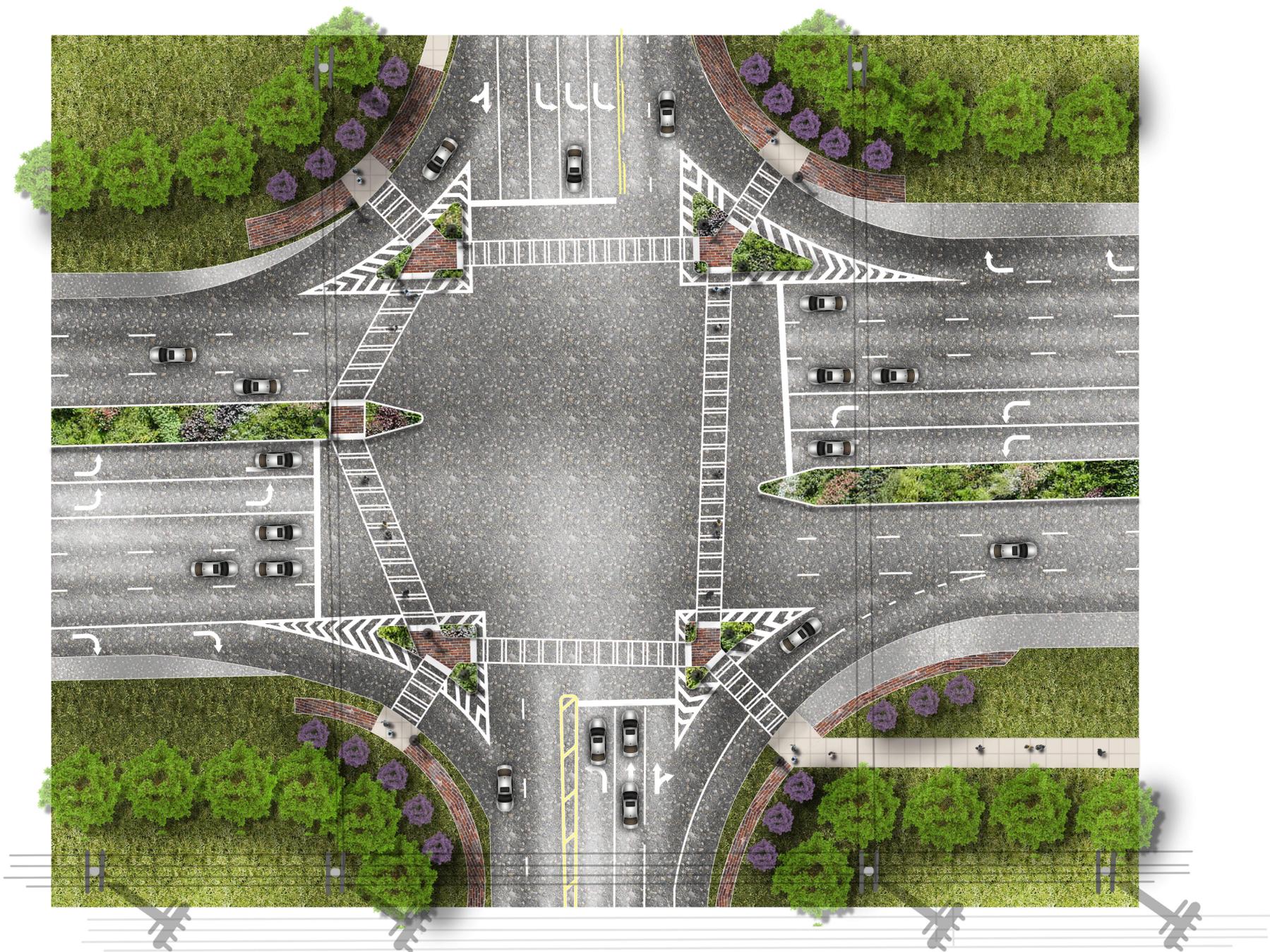
As requested, we have reviewed the Phase I Environmental Site Assessment Report that was written on September 11, 2019 and revised on October 29, 2019 by Contour Engineering of Kennesaw, Georgia. Our comments are as follows:

The Project Manager should provide a link of the accepted Report in ProjectWise to Geotechnical_Reports@dot.ga.gov.

If additional information is needed, please contact Jeffery Goodyear, P.E, of the Geotechnical Bureau at 404-608-4767 (Direct) or 404-608-4720 (Main).

MLF: GEF: JRG

cc: Troy Byers, State R/W Administrator
Attn: Hershel Thompson
Eric Duff, State Environmental Administrator
Joshua Higgins, District Planning and Programming Coordinator, Chamblee
District Right-of-Way Office, Chamblee
Jeanette L. Hamm, P.E., Contour Engineering, LLC
(jhamm@contouren.com)





DEPARTMENT OF PUBLIC WORKS

205 Lawrence St NE
P.O. Box 609
Marietta, GA 30061-0609
(770) 794-5650
Fax (770) 794-5585

4/3/20

Stephen Hopper
Stantec
229 Peachtree St NE, Suite 1900
Atlanta, GA 30303-1629

Re: PI0015049 Rottenwood Creek Phase 2 – Lighting Commitment

Dear Stephen,

This letter is to serve as a commitment by the City of Marietta to adding lighting to the Rottenwood Creek Phase 2 Trail. We understand that GDOT will not grant a design variance for lights in the 8' minimum set back and the Concept Report can reflect this. The City of Marietta will install lighting along the trail during or after construction depending on the availability of funds at that time.

Please feel free to reach out to me directly if you have any questions and I look forward to working with you and your team.

Sincerely,

Marc Simmons, PE
Transportation Engineer
City of Marietta Public Works
Transportation Program

ARTICLE XI

Exhibit A

MAINTENANCE WORK PLAN

for

SR 280,401

Rottenwood Creek Trail Phase II, PI 0015049

For all maintenance activities, at a minimum, abide by the Federal Manual of Uniform Traffic Control Devices (MUTCD) standards, current edition, for temporary traffic control. Move equipment or materials on or across a traveled way in a manner as not to unduly interfere with traffic.

Watering

- Provide adequate water to maintain healthy plant material
- Water in a manner that it does not endanger pedestrian or vehicular traffic
- Water according to the state or local government restrictions

Seasonal Color (Annuals and Herbaceous Perennials and Roses)

- Install and maintain plant material at a height that does not interfere with clear sight lines for both pedestrians and vehicular traffic according to the Department's sight distance criteria
- Maintain bare areas in seasonal beds with a minimum 3 inch cover of mulch
- Cut back perennials each year after they are spent to keep the beds free of vegetative debris
- Maintain any rose shrubs at a height that will not obstruct existing directional signs or driver sight lines.
- Monitor shrub roses for Rose Rosette Virus. Remove roses that exhibit symptoms and consider removing all rose plantings in a bed where RRV is observed.

Ornamental Grasses Trim away dead foliage from ornamental grass clumps in February.

Pruning

- Remove dead or diseased planted vegetation.
- Prune trees, shrubs and ground covers to maintain the health of the plants and to maintain in the intended design character of the plant (no stump pruning or lollipop/ball shapes)
- Prune trees, shrubs, and ground covers as needed to remove damage by storm or accident events and to prevent safety hazards. Prune to maintain open sight distances, clear zone areas and traffic sign visibility. Provide clearance for pedestrian and vehicular traffic mobility.
- Prune according to American National Standards Institute, latest edition, A300 Part 1 pruning standards

Plant Replacement

- Replacement of dead or diseased vegetation of planted material within the project limits is the responsibility of the LICENSEE
- Replacement plant material must be according to Policy 6755-9 – Policy for Landscaping and Enhancements on GDOT Right of Way and GDOT Specification Section 702.

Weeding

- Maintain right of way free of weeds, exotic and invasive pest plants, undesired vegetation and other noxious weeds.
- Any herbicides used shall be approved by the City of Marietta and the Georgia Department of Transportation.
- Applications of herbicide shall conform to GDOT guidelines and will require approval from/coordination with GDOT Office of Maintenance Agronomist Manager.
- All Herbicide use shall be under the direct supervision of someone with the appropriate Commercial Category 27 (right of way use) license.
- When herbicides are being applied the person applying shall have in their possession all labeling associated with the pesticide/herbicide and their license/certification.

- Post warning signs for herbicide use as required by state code.

Pest Control

- Pest management shall be addressed by Integrated Pest Management (IPM) techniques.
- Any pesticides used shall be approved by the City of Marietta and the Georgia Department of Transportation.
- Applications of pesticide shall conform to GDOT guidelines and will require approval from/coordination with GDOT Office of Maintenance Agronomist Manager.
- All Pesticide use shall be under the direct supervision of someone with the appropriate Commercial Category 27 (right of way use) license.
- When pesticides are being applied the person applying shall have in their possession all labeling associated with the pesticide and their license/certification.
- Post warning signs for pesticide use as required by state code.

Mowing and trimming of grass

- Maintain a neat appearance and clear sight lines for pedestrian and vehicular traffic.

Mulching

- Replace mulch in plant beds as needed to maintain an attractive, fresh look at a 2-3" depth
- Maintain mulch so that it will not spread or wash on to pedestrian paths or traveled lanes

Litter

- Completely remove all litter and debris and other objectionable material on site.
- Do not deposit or blow litter, debris and vegetation into gutters or drainage structures.
- Make disposal in accordance with local and state laws.
- Remove all graffiti within project limits

Installed Sidewalks

- Maintain and repair sidewalks according to the Americans With Disabilities Act (ADA)

Installed Non-standard Fencing/Site Furnishings/Murals/Signs/Walls Repair and/or replace damaged components.

- Replace or install a temporary construction work fence immediately if the damage involves a safety issue.
LICENSEE is responsible for all maintenance of non-standard fencing/site furnishings/murals/signs

Installed Low Voltage Lighting

- Repair and/or replace damaged components. Repair and/or replacement must be done immediately if the damage involves a safety issue.
- LICENSEE is responsible for all maintenance of low voltage lighting.

NOTE:

All major maintenance repair activities and activities that may interfere with traffic or pedestrian flow within the right of way project limits, such as travel lane/walkway closures, require the LICENSEE notify the Department at least 48 hours prior to the activity to coordinate and gain Department approval.

From: [Anninos, Jack A](#)
To: [Adekonojo, Olusola T](#)
Cc: [Zehngraff, Scott E.](#); [Weiss, Megan J](#)
Subject: Re: 0015049 - Cobb County, City of Marietta - Rottenwood Creek Trail Phase 11 – Scoping Only - Project Justification Statement
Date: Monday, August 3, 2020 9:21:19 PM
Attachments: [image001.png](#)
[image002.png](#)

Sola -

I have reviewed the Project Justification Statement and it looks good from bike/ped perspective.

Please let me know if you have any further questions. Thank you!

Jack Anninos
State Bicycle & Pedestrian Engineer
GDOT Traffic Operations
703-861-8611

From: Adekonojo, Olusola T <OAdekonojo@dot.ga.gov>
Sent: Monday, July 20, 2020 02:42 PM
To: Anninos, Jack A <JAnninos@dot.ga.gov>
Cc: Zehngraff, Scott E. <szehngraff@dot.ga.gov>
Subject: FW: 0015049 - Cobb County, City of Marietta - Rottenwood Creek Trail Phase 11 – Scoping Only - Project Justification Statement

Good day Jack,

Please do you have a status per this request, Office of Planning forwarded it 04/13/2020

Regards.

Sola Adekonojo, PMP
D7B Project Manager



Office of Program Delivery
600 West Peachtree Street, 25th Floor
Atlanta, GA 30308
Office: 404-631-1545
Cell: 470-633-7433
Fax: 404-631-1588
Email: oadekonojo@dot.ga.gov

From: Weiss, Megan J <MWeiss@dot.ga.gov>